



US Army Corps
of Engineers

MISCELLANEOUS PAPER GL-90-13



CONDITION SURVEY AND PAVER IMPLEMENTATION CANNON AIR FORCE BASE, NEW MEXICO

by

William P. Grogan

Geotechnical Laboratory

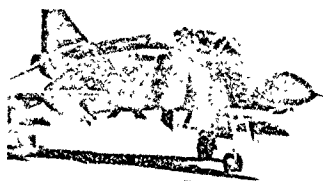
DEPARTMENT OF THE ARMY

Waterways Experiment Station, Corps of Engineers
3909 Halls Ferry Road, Vicksburg, Mississippi 39180-6199

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| PAVEMENT CONDITION INDEX (PCI) | PAVEMENT CONDITION RATING |
|-----------------------------------|------------------------------|
| 100 | EXCELLEN |
| 90 | VERY GOOD |
| 80 | GOOD |
| 70 | FAIR |
| 60 | POOR |
| 50 | VERY POOR |
| 40 | FAUL |

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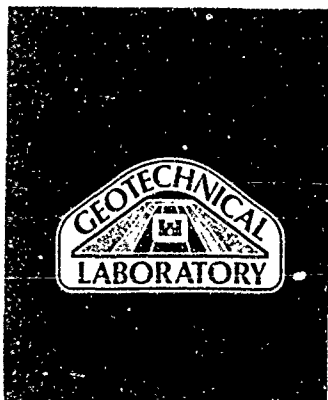


August 1990

Final Report

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PREFACE

The condition survey described in this report was requested by Military Interdepartmental Purchase Request (MIPR) No. N589-119 dated 7 April 1989 from the Headquarters TAC/DEPF, Langley Air Force Base, VA, to the US Army Engineer Waterways Experiment Station (WES), Vicksburg, MS.

The condition survey at Cannon Air Force Base was performed by a WES condition survey team during the period 1 to 8 June 1989. The team consisted of Messrs. S. J. Alford, R. H. Barlow, Jr., W. P. Grogan, and D. D. Mathews, Pavement Systems Division (PSD), Geotechnical Laboratory (GL). This report was prepared by Mr. Grogan under the supervision of Mr. H. H. Ulery, Jr., Chief, PSD. The work was under the general supervision of Dr. W. F. Marcuson III, Chief, GL, WES. Ms. Odell F. Allen, Visual Production Center, Information Technology Laboratory, edited the report.

COL Larry B. Fulton, EN, was the Commander and Director during the preparation and publication of this report. Dr. Robert W. Whalin was the Technical Director.



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CONVERSION FACTORS, NON-SI TO SI (METRIC)
UNITS OF MEASUREMENT

Non-SI units of measurement used in this report can be converted to SI
(metric) units as follows:

| <u>Multiply</u> | <u>By</u> | <u>To Obtain</u> |
|-----------------|------------|------------------|
| feet | 0.3048 | metres |
| inches | 2.54 | centimetres |
| square feet | 0.09290304 | square metres |
| square yards | 0.8361274 | square metres |

CONDITION SURVEY AND PAVER IMPLEMENTATION

CANNON AIR FORCE BASE, NEW MEXICO

PART I: INTRODUCTION

Background

1. This report describes the condition survey and initial implementation of a pavement management system using the PAVER system of the airfield pavements at Cannon Air Force Base (AFB), NM. The implementation was performed to provide base engineers with the initial data base required for making pavement management decisions concerning costs and maintenance requirements. The condition survey was performed by the US Army Engineer Waterways Experiment Station during the period 1 to 8 June 1989.)

Objective and Scope

2. The overall objective of this project was to determine the pavement condition of the airfield pavements at Cannon AFB and to input the information into a PAVER data base to provide the base engineers with a permanent data base to use for future pavement management decisions. This objective was accomplished by:

- a. Performing a condition survey of the pavements in accordance with AFR 93-5)*
- b. Inputting the pavement network and condition survey information into PAVER to calculate a pavement condition index (PCI) of each of the pavement features;
- c. Completing the data base implementation by compiling pavement construction data and inputting the information into the PAVER data base; AND
- d. Producing detail drawings of the pavement features to ensure that future condition surveys will be performed at the same locations as the one performed for this report.

* Headquarters, Department of the Air Force. 1981 (May). "Airfield Pavement Evaluation Program," Air Force Regulation AFR 93-5, Washington, DC.

PART II: PAVEMENT CONDITION SURVEY

Introduction

3. An airfield pavement condition survey determines the present surface condition of the airfield pavements. The procedure used in performing the condition survey was developed by the US Army Corps of Engineers and has been accepted as a regulation by the US Air Force. Knowledge of the condition survey procedures discussed in AFR 93-5* is beneficial for the use and understanding of this report.

Pavement Definition and Identification

4. The pavement network is divided into three specific units in order to manage the pavement network effectively. The three units of division are the branch, the section, and the sample unit. The method for dividing the pavement network is detailed in AFR 93-5 and is briefly discussed herein.

5. Airfield pavement branches (or features) are defined by parameters such as the pavement type, construction history, and pavement usage. The branch designations used for Cannon AFB were established in the 1987 report "Airfield Pavement Evaluation, Cannon Air Force Base, New Mexico."** The branch designations, shown in Figure 1, are indicative of pavements subjected to similar traffic with like cross sections.

6. After each pavement branch has been defined, further division of the branch may be required for reasons such as traffic flow. Further division of branches is done into sections. For instance, a runway branch may be 150 ft† wide, but the majority of the traffic occurs in the middle of the branch. Therefore, a section is defined in the center of the branch with additional sections defined on either side of the middle section. The runways at Cannon AFB were divided into three sections. An apron may contain taxi lanes which

* Headquarters, Department of the Air Force. 1981. "Airfield Pavement Evaluation Program," Air Force Regulation AFR 93-5, Washington, DC.

** Air Force Engineering and Services Center. 1987 (May). "Airfield Pavement Evaluation Report, Cannon Air Force Base, New Mexico," Tyndall AFB, FL.

† A table of factors for converting non-SI units of measurement to SI (metric) units is presented on page 3.

the aircraft follow to their parking locations, a section which would differ from the areas used for the actual parking of the aircraft. Therefore, these elements of the branch may be divided into sections. If a branch requires no division, it is defined as containing one section.

7. After the pavement section definition has been completed, the section is divided into sample units. Sample units are conveniently sized areas of pavement on which the inspection is performed. A sample unit on asphaltic concrete (AC) pavement is a 5,000-sq ft area, and a sample unit on portland cement concrete (PCC) pavement consists of 20 slabs. A pavement section is divided into sample units for condition survey purposes only. Recognizing that not all sample units can be 5,000 sq ft or 20 slabs, deviations of 50 percent on either side of these values may be used for survey purposes.

8. When a section has been divided into sample units, it is ready to be surveyed. Inspection of all of the sample units within a section could require a considerable amount of time. Therefore, the random sampling method was developed to provide an adequate calculation of the PCI while inspecting only a portion of the sample units in a section. The method, further defined in AFR 93-5, allows for a reduction in the number of sample units surveyed without a significant loss of accuracy in the calculation of the PCI. It should be noted, however, that the inspection of all the sample units may be necessary for the estimation of maintenance and repair work.

9. An essential concept in pavement management is determining the deterioration of the pavement surface over time. The PCI is used in the PAVER system to determine this deterioration. Determining the PCI of a pavement section at different time intervals using the same sample units of the section in each survey gives a precise idea of the deterioration rate. Drawings of each pavement branch with sample unit locations illustrated are included in Figures 2 through 10. The locations of the sample units in the asphalt runway and taxiway branches were made using stationing. Referencing of the PCC branches including taxiways, aprons, and runways were made with respect to the location of the slabs in each branch. The sample units to be surveyed were located by counting slabs. The circled numbers indicate the sample units that were surveyed. The match lines on the drawings are keyed to the sheet number in the lower right hand corner of Figures 2 through 10.

Pavement Inspection

10. The performance of a condition survey consists of inspecting the pavement surface for various types of distresses, determining the severity of each distress found, and measuring the amount of distress within the sample unit. Distress quantities on AC pavement are measured in either linear feet or square feet within the sample unit. PCC pavement distresses are measured by counting the number of slabs affected within the sample unit.

11. The result of the condition survey is the PCI of the sample unit. The PCI is a value from 0 to 100 reflecting the condition of the pavement surface. The numerical index of the PCI ranges from 0-10, 11-25, 26-40, 41-55, 56-70, 71-85, and 86-100 corresponding to the PCI ratings of failed, very poor, poor, fair, good, very good, and excellent, respectively. The PCI is obtained by determining a deduct value for the amount and severity level of each distress type found in a sample unit. A correct deduct value is determined for the combined effect of various distresses on the pavement condition. The corrected deduct value is subtracted from 100, resulting in the PCI value. A sample unit with no distress has a PCI of 100 with varying amounts of distress decreasing the PCI value to a possible low of 0. The PCI of the pavement section is calculated by averaging the PCI's of the sample units surveyed in the respective section.

12. The majority of the pavement branches at Cannon AFB are rated from good to excellent conditions with some branches rated from very poor to fair. Figure 11 illustrates the condition ratings of the features at Cannon AFB. Photos 1 through 30 show various distresses that were observed on the airfield pavements.

PART III: PAVER DATA BASE IMPLEMENTATION

Introduction

13. The use of the PAVER system requires knowledge of both computers and the PAVER system. This report does not describe the operation of a computer; it does outline the necessary PAVER procedures in moderate detail. The "PAVER User's Guide"* goes into specific detail of all the procedures for setting up and using a PAVER data base and should be used as a reference when performing operations in the PAVER system.

14. The PAVER system consists of five different system functions. Performing each function requires the use of specific programs, files, and procedures. The five functions are data entry, system sign-on, data base update, report generation, and data analysis. Data entry, system sign-on, and data analysis do not directly interact with the PAVER data base, but data base update and report generation require data base interaction.

Data Entry

15. The pavement network data are entered into the PAVER data base in a logical order that defines the branches and sections first. The additional information that allows the user to perform data base related operations such as PCI calculation and report generation is then entered. The data must be in specific formats for it to be accepted by the data base. Three data input programs are used to prepare data for the specific formats: PAVERIN, EDITOR, and REFORMT. All of these programs have been written in the BASIC computer language and are operable on a personal computer that contains a BASIC system. The PAVERIN program is used to input the data into the correct formats; the EDITOR program is used for editing any errors that may have been placed in the data, and the REFORMT program is used to prepare the data for uploading onto the mainframe computer.

16. The condition survey data are collected in the field by recording the data manually on condition survey data sheets, or by inputting the data

* Shahin, M. Y. 1985. "The PAVER User's Guide," ADP-356-1, US Army Construction Engineering Research Laboratory and US Army Facilities Engineering Support Agency.

directly into the FIELD program on a portable computer. The FIELD program places the data into PAVER format as the data are entered into the computer and saves the data in a file that can be directly uploaded to the mainframe computer. If the data are collected manually on survey sheets, the PAVERIN data input program is used to enter the data in the PAVER system. The data for Cannon AFB were collected and compiled on data sheets at the time of the survey and later input into PAVERIN.

17. The data for physical properties and construction history of the pavements at Cannon AFB were obtained from the 1987 evaluation report and from base engineering personnel. The physical property data were entered into the data base.

System Sign-On

18. The mainframe PAVER system currently resides on a Control Data Corporation (CDC) computer and is accessible through a remote terminal via a telephone link. The telephone link is achieved by using a modem and appropriate communication software. Connection to the system requires dialing the CDC computer for connection and then entering the appropriate access codes to sign-on the computer. The access codes (user ID, password, and charge number) are obtained when a charge account has been set up with CDC.

Data Upload and Data Base Update

19. Data are added to the data base either interactively or by using the BATCH method. The interactive method is used when the user is on-line to the CDC computer. This method is easier to perform but is more expensive. The BATCH involves transferring the data file created with the PAVERIN or FIELD programs from the personal computer to the CDC mainframe. Using either operation involves creating the file DATAFL on the CDC computer from which the data are read into the data base. After DATAFL is prepared, the PAVER system checks it for errors, and after corrections have been made, the data are loaded into the data base.

Report Generation and Data Analysis

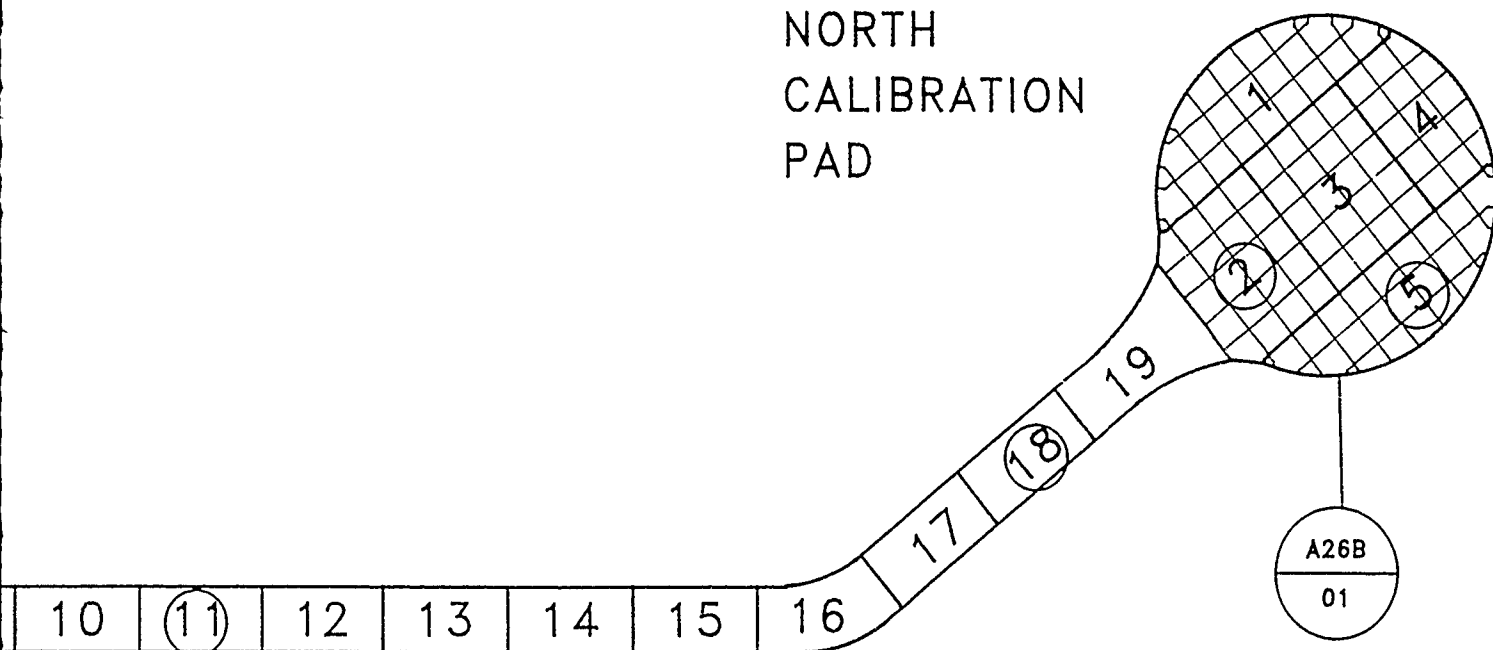
20. The PAVER system generates reports that provide a summary or specific information based on the data stored in the mainframe data base. It also calculates information such as budget needs from data and analysis programs provided by PAVER. These reports can be generated either interactively or through a BATCH process. The BATCH process produces the report when the user is not signed-on to the CDC computer and is more cost-effective when generating large amounts of information. The interactive process, performed while the user is signed-on, can be used effectively when generating smaller reports and detecting data base errors.

21. There are two types of data analysis programs in the PAVER system: those that access the data base and those that do not access the data base. The difference in the two types is that the data base must be on line for the report to operate. The user responds to questions that the program asks, and then analysis results are produced based on those responses. The analysis reports can only be generated using the interactive process.

22. The data reports and analysis programs provide an engineer with the information required to make pavement management decisions. Those reports (LIST, INV, PCI, SCHED, and SAMPCUR) are found in Appendix A.

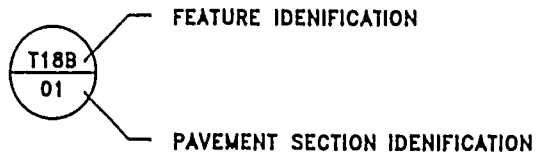
23. The LIST report lists the branch number, branch name, and number of sections in each branch. The INV report provides inventory information of each pavement section including surface type, branch use, pavement rank, and section area in square yards. The PCI report provides a list of the PCI's of each section ranked by PCI from low to high. The report SCHED gives a schedule of sections to be inspected in the next 5 years. The SAMPCUR report lists the summary and sample unit PCI and distress information on each pavement section in the data base.

NORTH CALIBRATION PAD



ON PAD ACCESS

LEDGEND



SHEET 9

Sample unit locations on Branches R01A, R02C, T11A,
A01B, A12B, A13B, O03C, and O04C

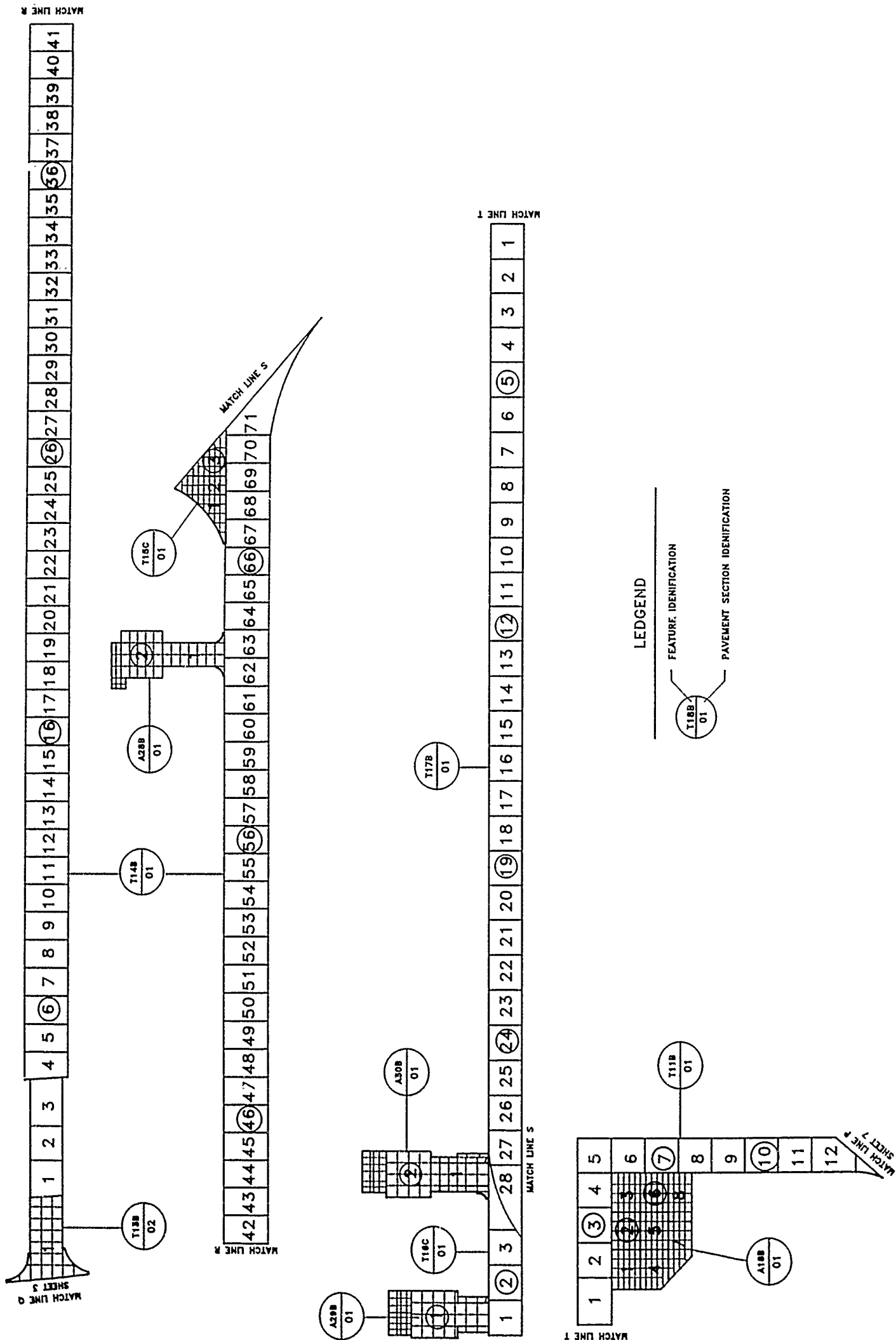


Figure 3. Sample unit locations on Branches R02C, R06C, R07C, T09A, T10A, T11A, T19A, T20A, T22A, A01B, A02B, A03B, A08B, A31B, and ROAD

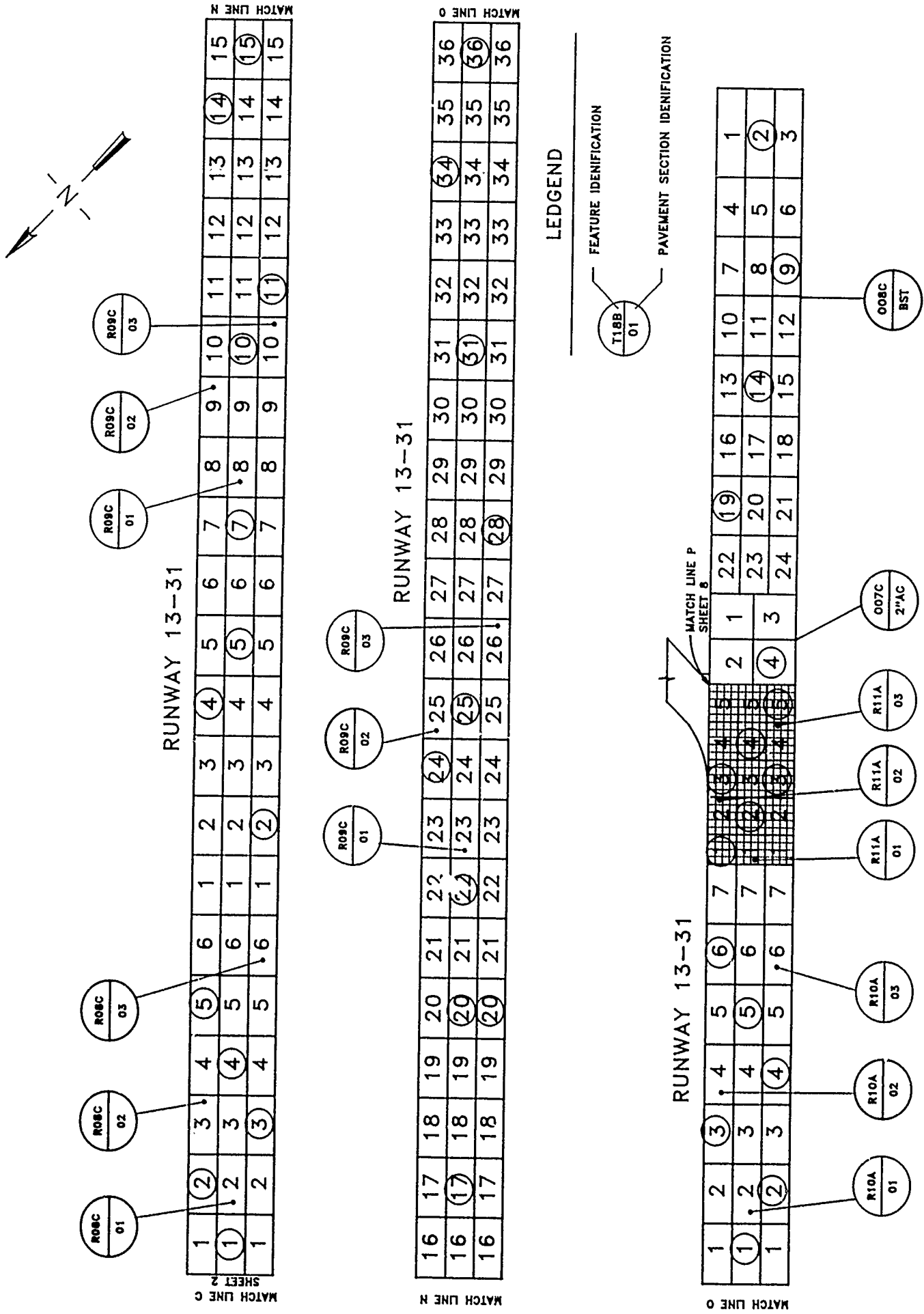


Figure 4. Sample unit locations on Branches R02C, T12B, T13B, T20A, A02B, A03B, A08B, A09B, A27B, and ROAD

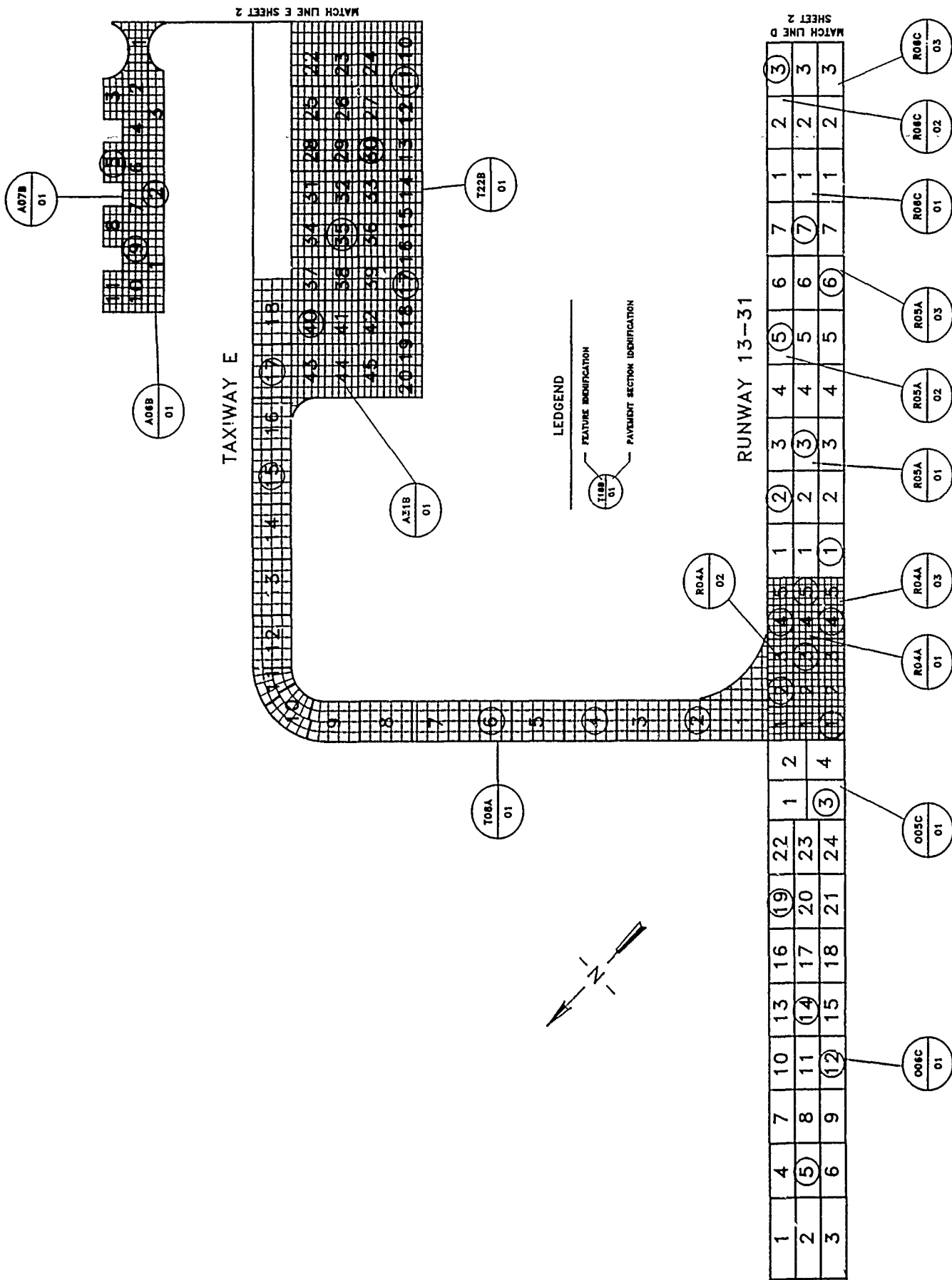


Figure 5. Sample unit locations on Branches R02C, T03A, T05B, T06B, T20A, A02B, A03B, A04B, A05B, A09B, A10B, and ROAD

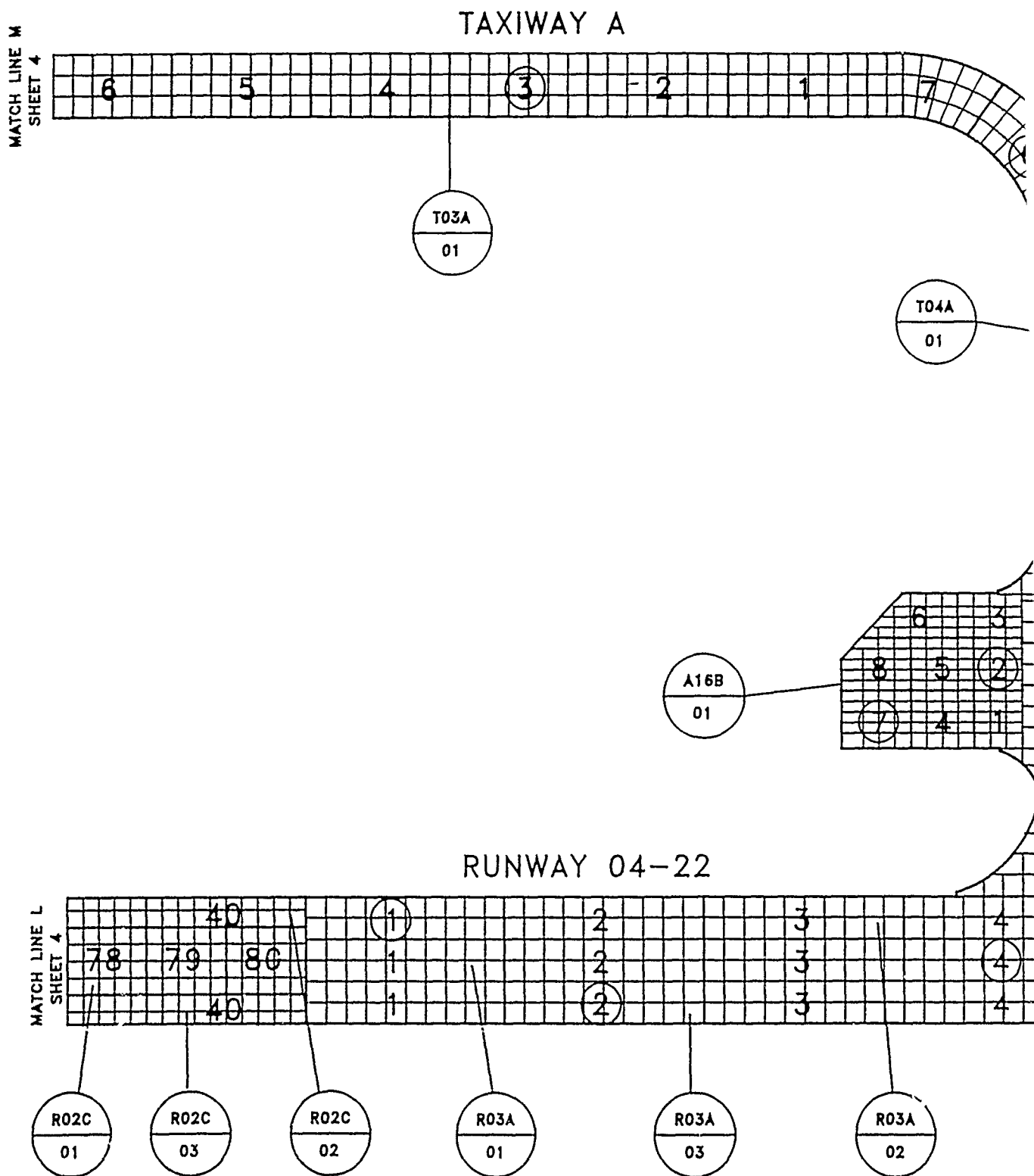


Figure 6. Sample unit
T04A, A14E

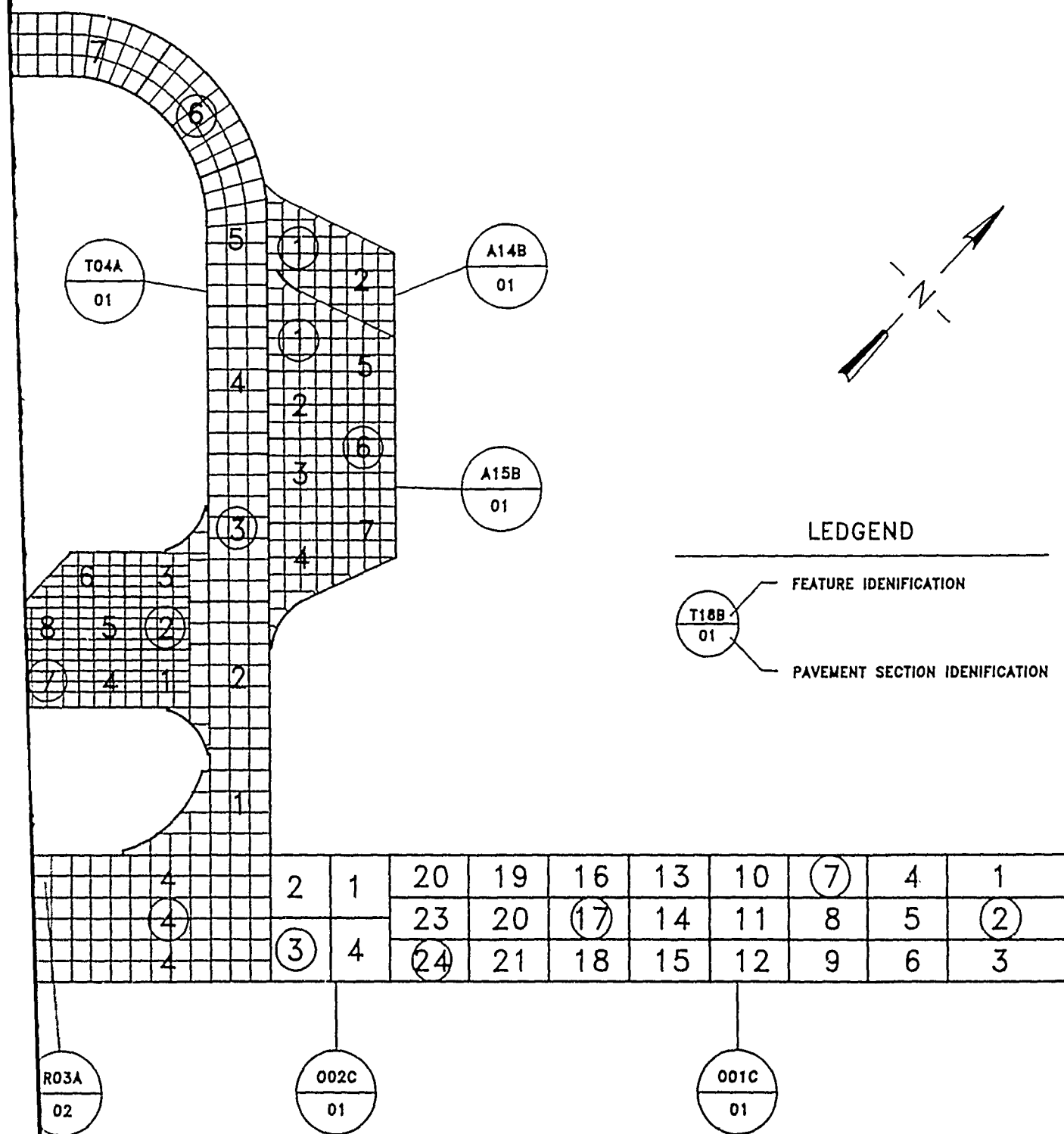


Figure 6. Sample unit locations on Branches R02C, R03A, T03A, T04A, A14B, A15B, A15B, O01C, and O02C

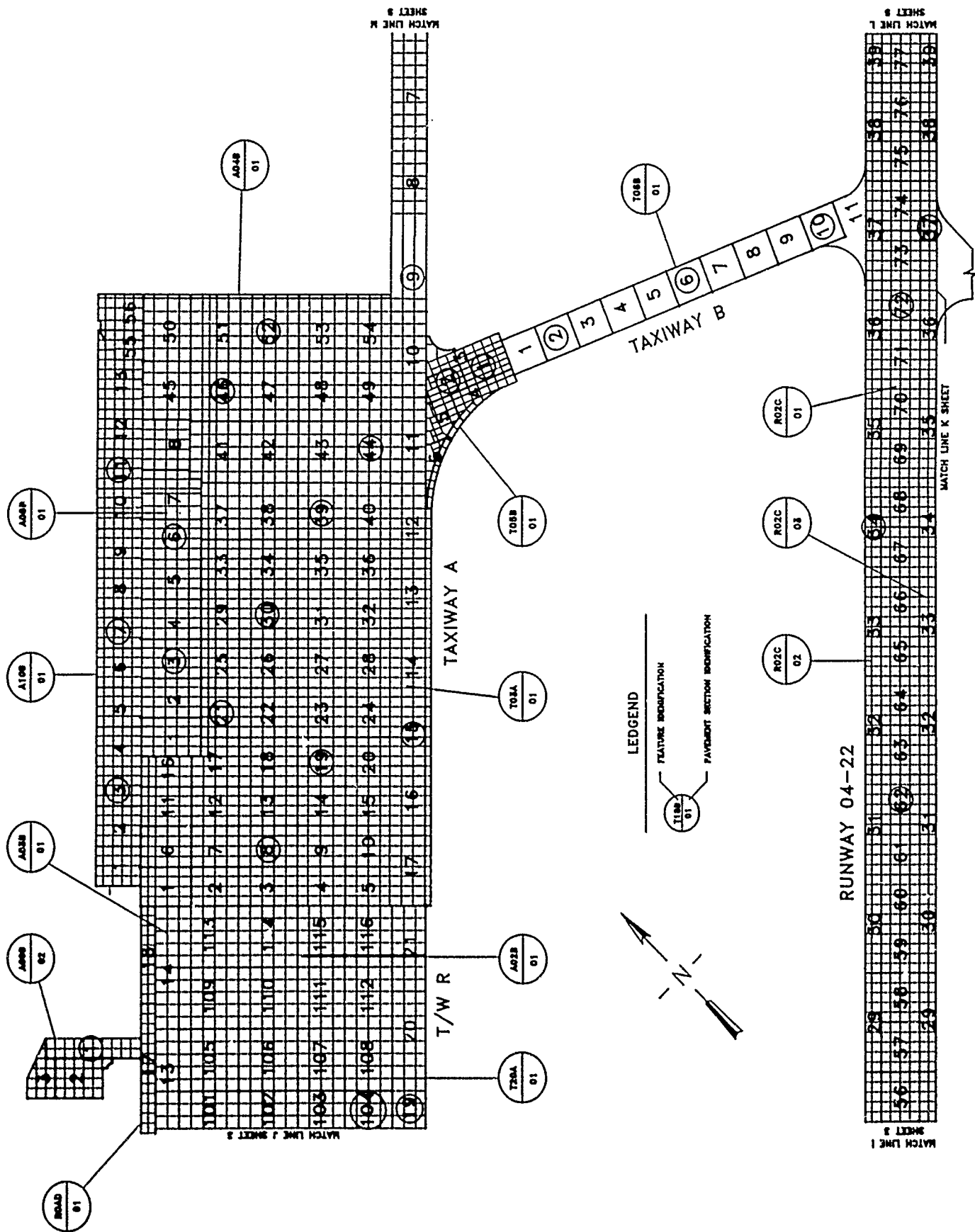


Figure 7. Sample unit locations on Branches R04A, R05A, R06C, T08A, T22B, A06B, A07B, A31B, O05C, and O06C

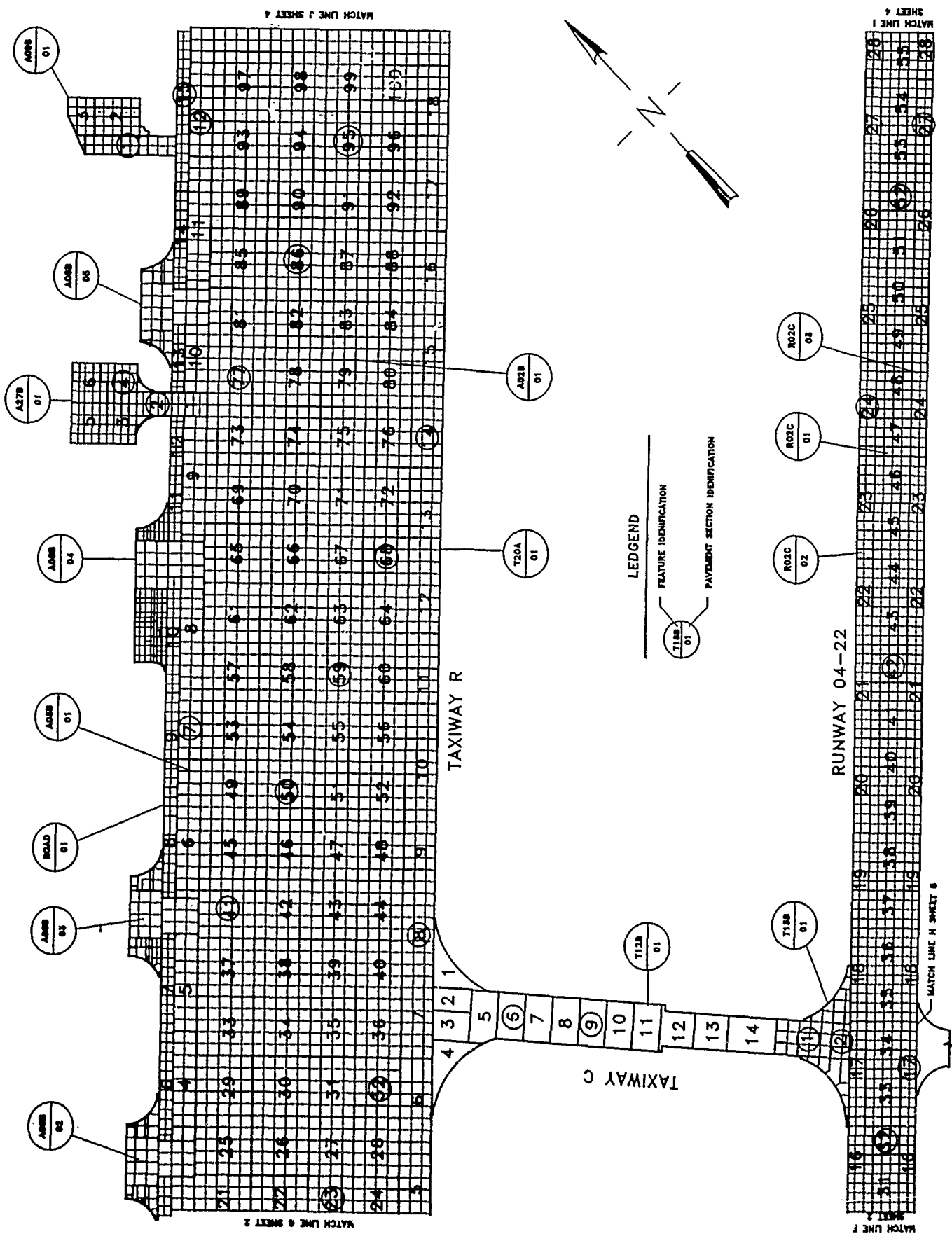


Figure 8. Sample unit locations on Branches R08C, R09C, R10A, R11A, O07C, and O08C

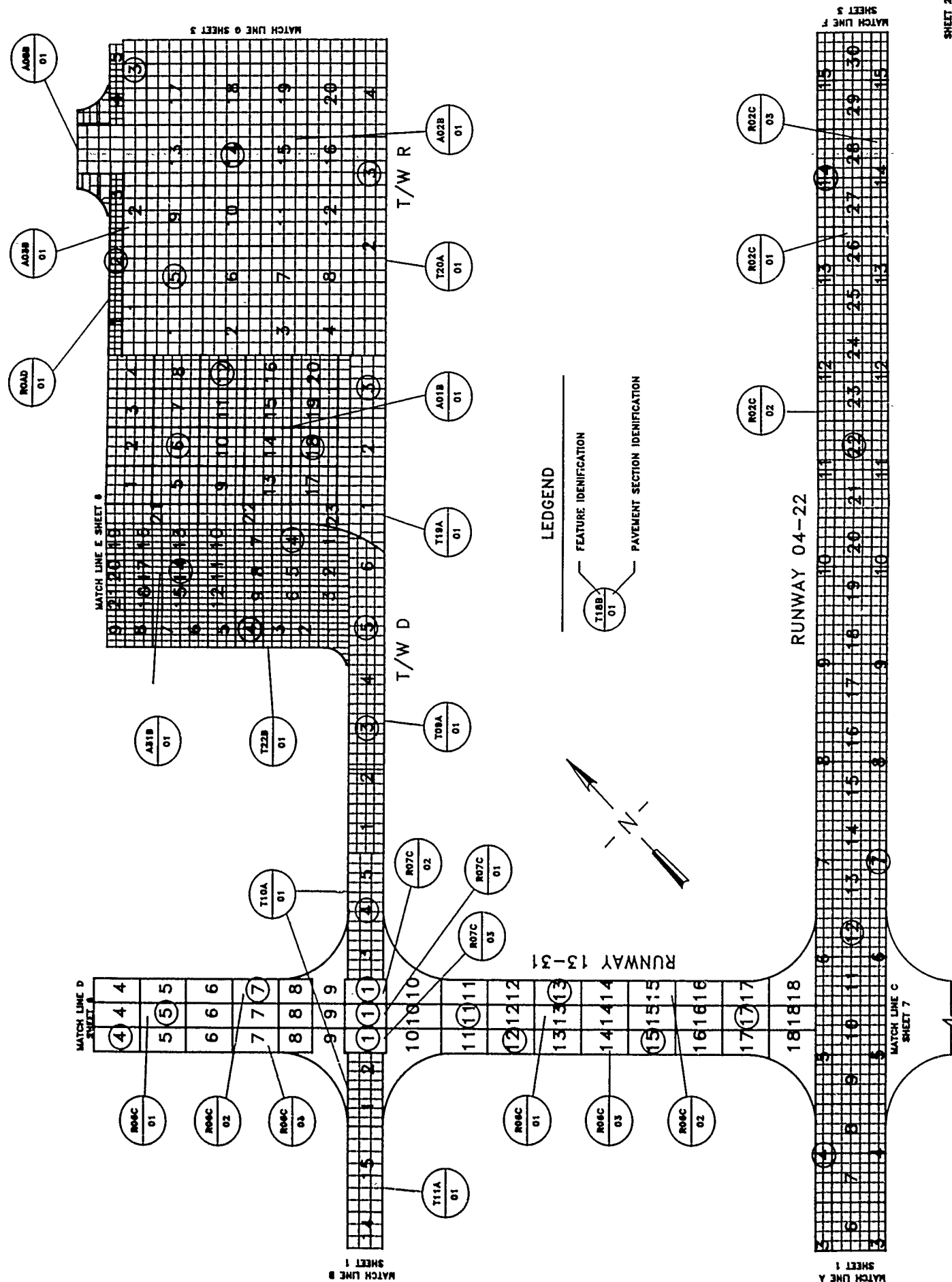


Figure 9. Sample unit locations on Branches T11B, T13B, T14B, T15C, T16C, T17B, A18B, A28B, A29B, and A30B

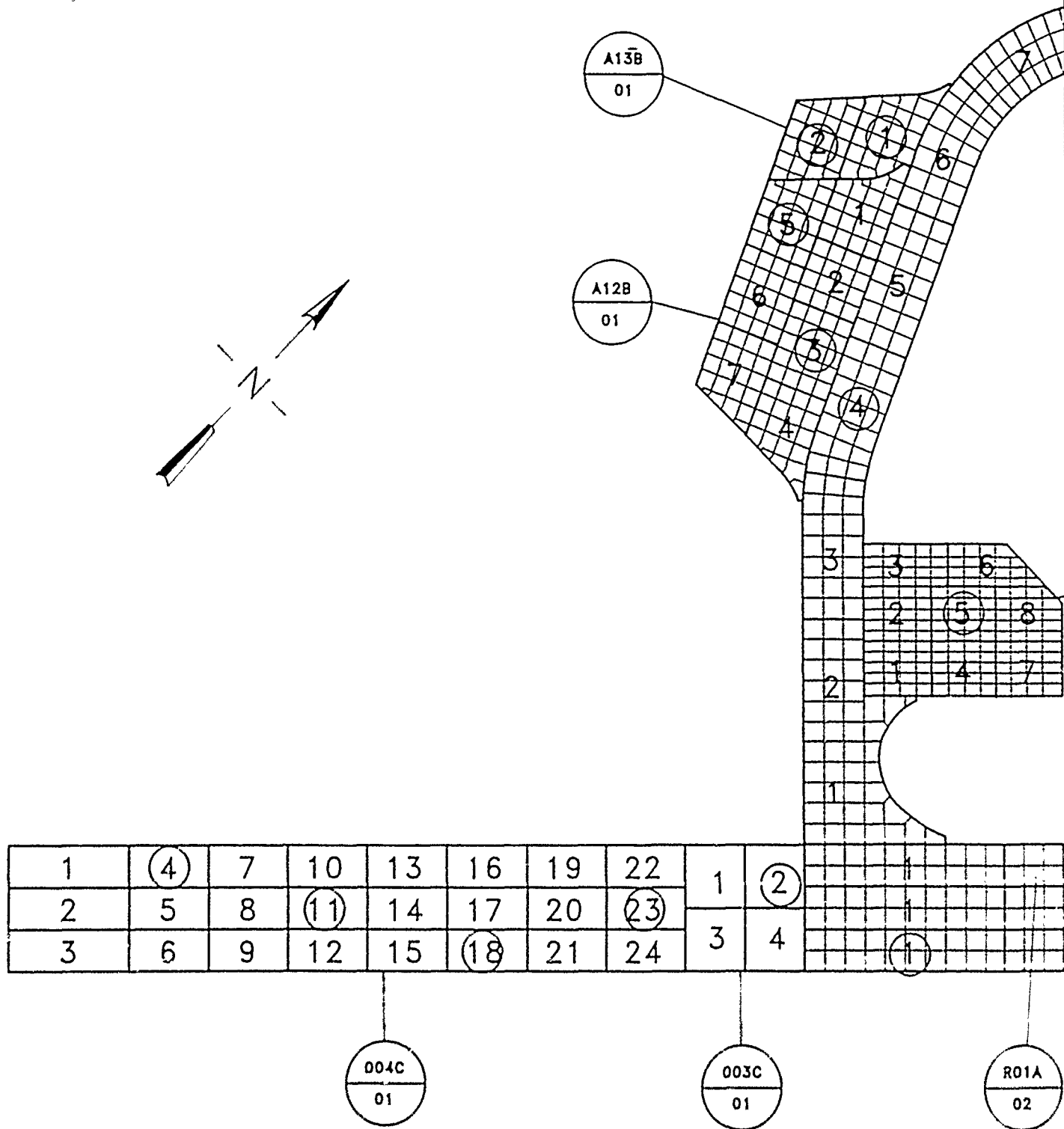


Figure 10. Sample unit

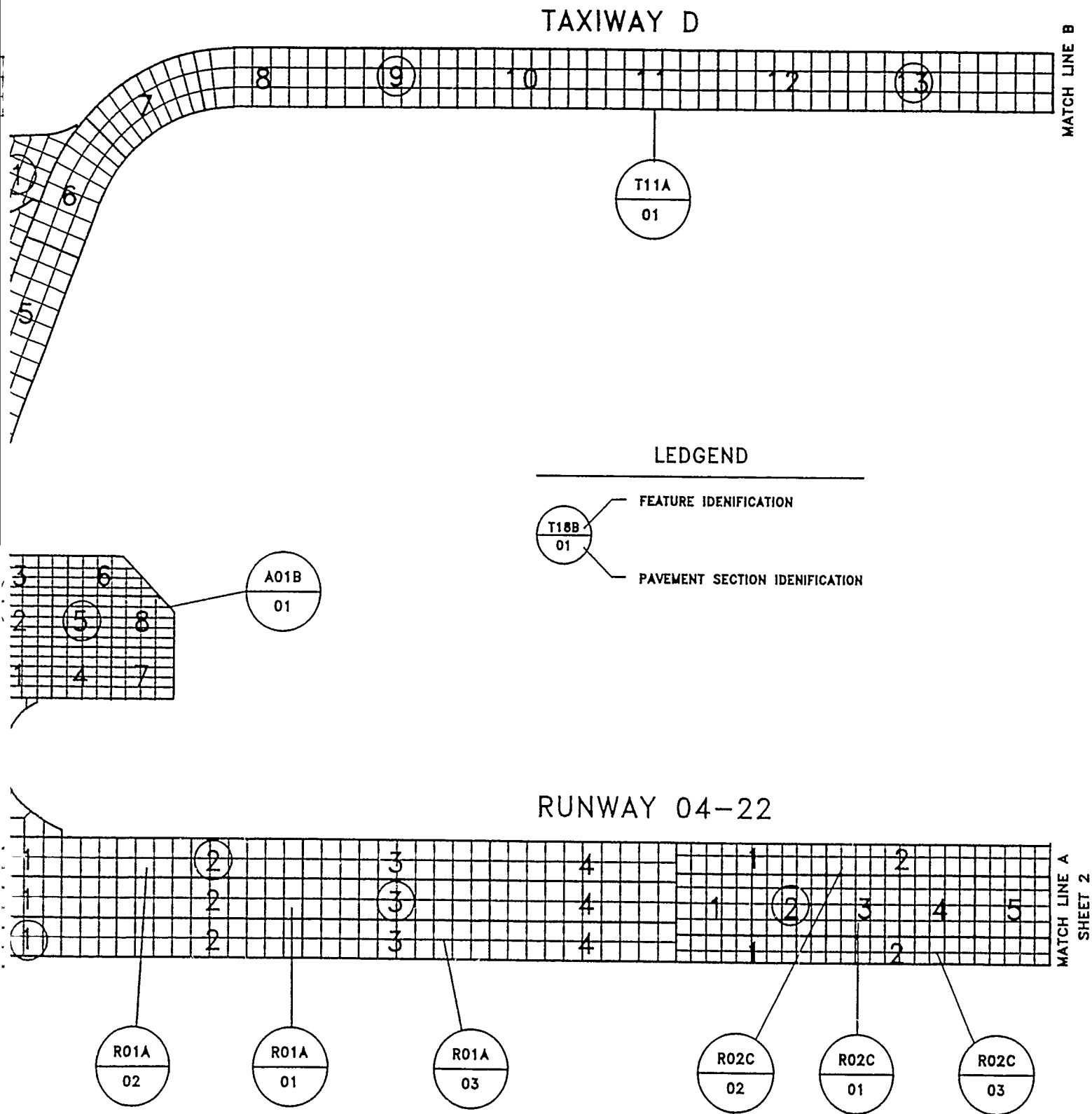


Figure 10. Sample unit locations on Branches T07C and A26B

SHEET 1

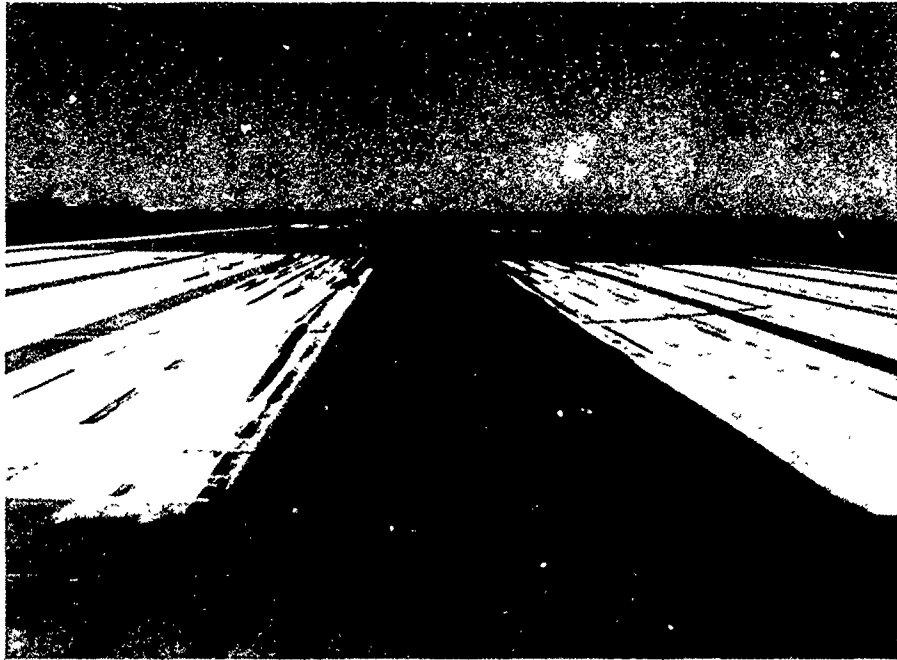


Photo 1. Overall view of R01A



Photo 2. Small Patch on R01A

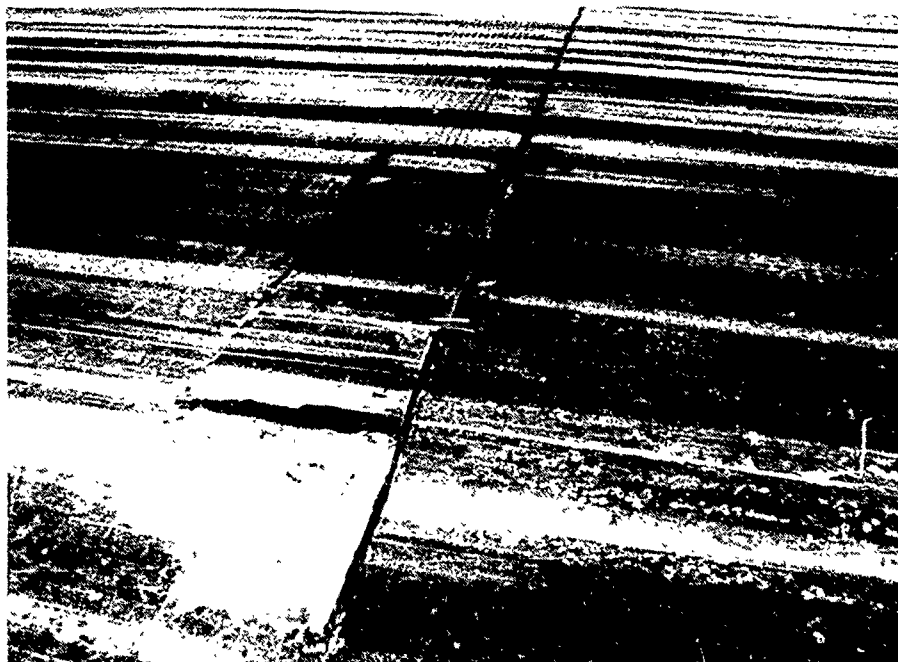


Photo 3. Large patch on R02C

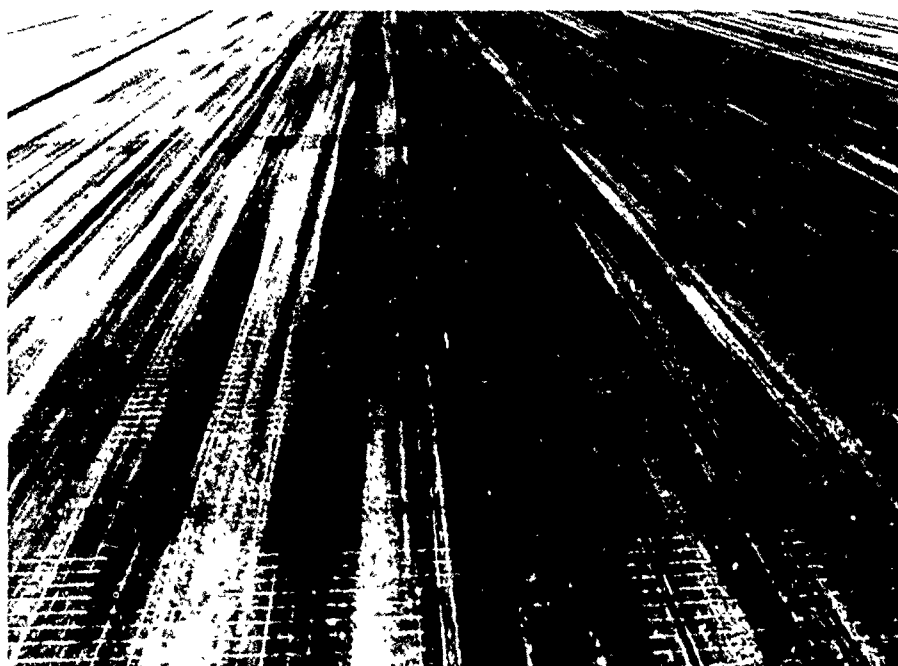


Photo 4. Center-line view of R02C

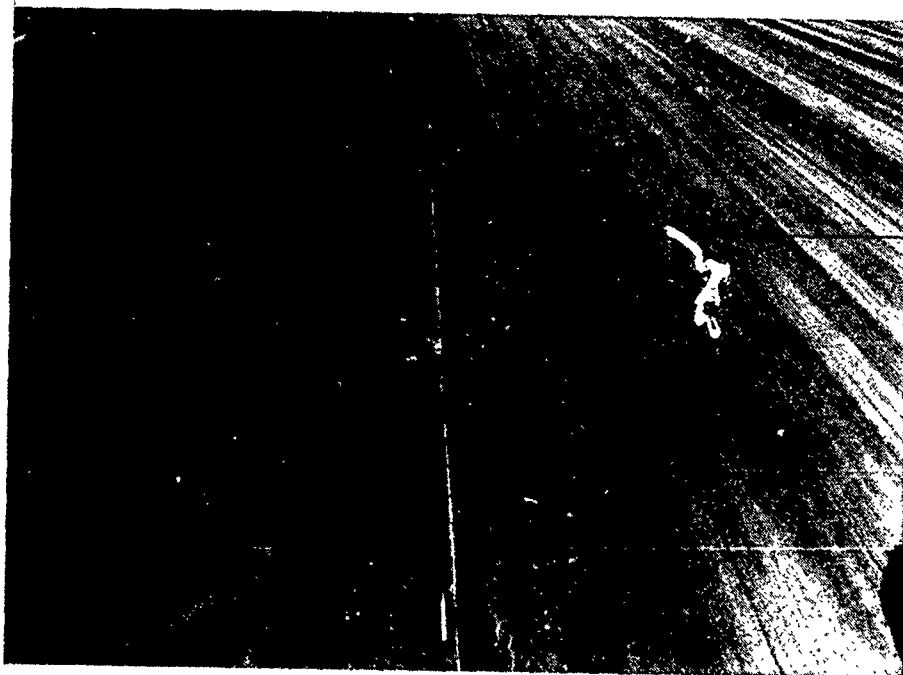


Photo 5. Center-line view of R03A



Photo 6. Overall view of R11A



Photo 7. Sealed longitudinal/transverse crack
on R11A



Photo 8. Overall view of R10A

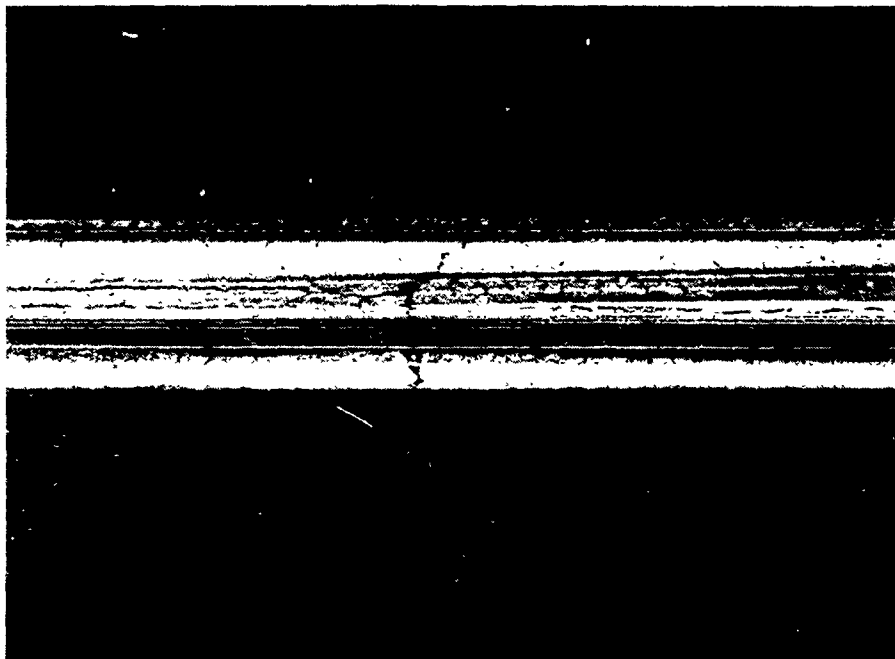


Photo 9. Longitudinal/transverse crack on R10A

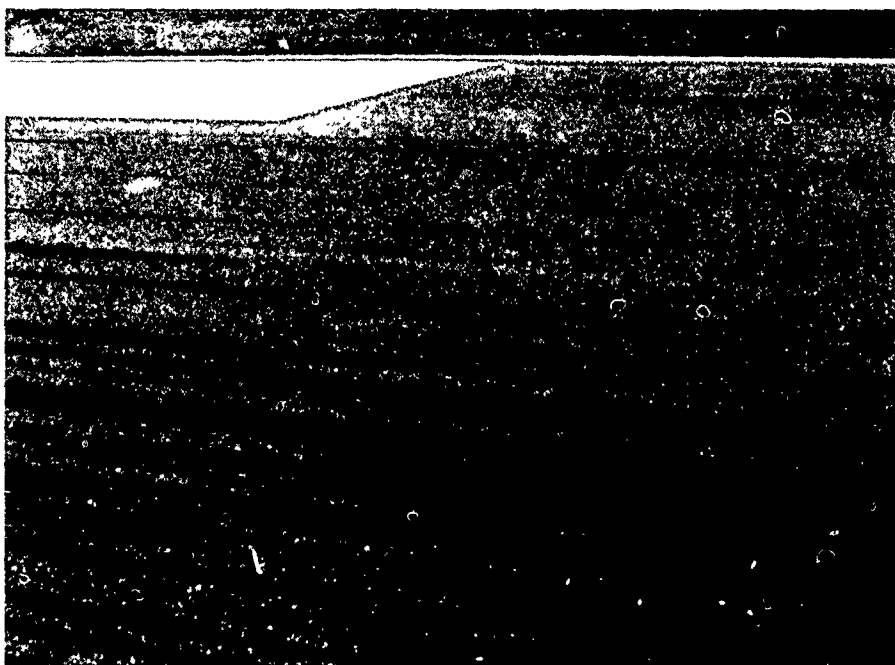


Photo 10. Low-severity transverse crack on R10A

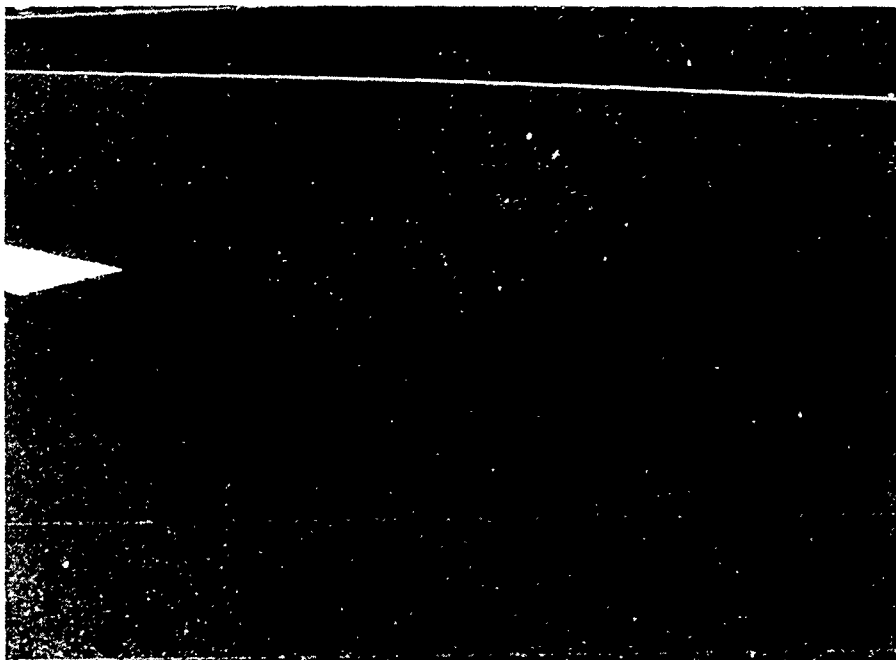


Photo 11. No distress on R9C

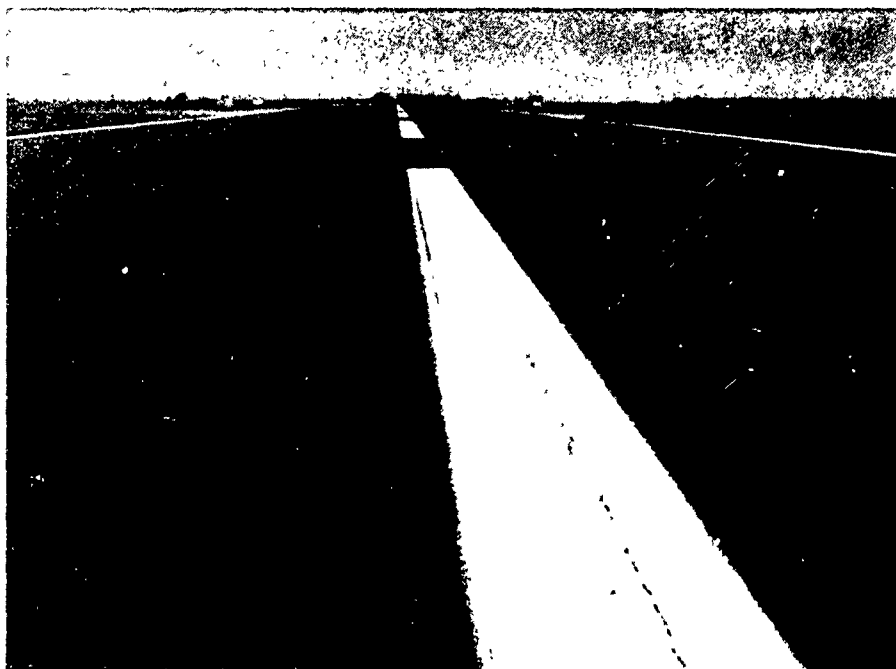


Photo 12. Overall view of R9C



Photo 13. Overall view of R4A



Photo 14. Failing spall patch on R4A



Photo 15. Sealed longitudinal crack on R4A



Photo 16. Patched corners on ROAD

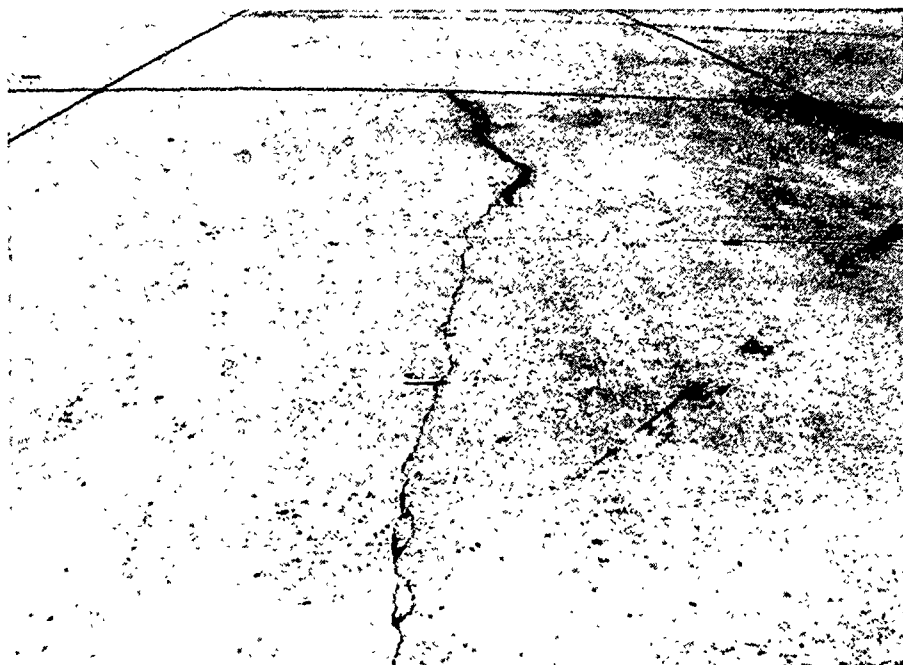


Photo 17. Longitudinal/transverse crack on A02B



Photo 18. Sealed longitudinal/transverse crack
on T3A



Photo 19. Center slabs replaced on T3A



Photo 20. Patched and sealed longitudinal crack
on T3A

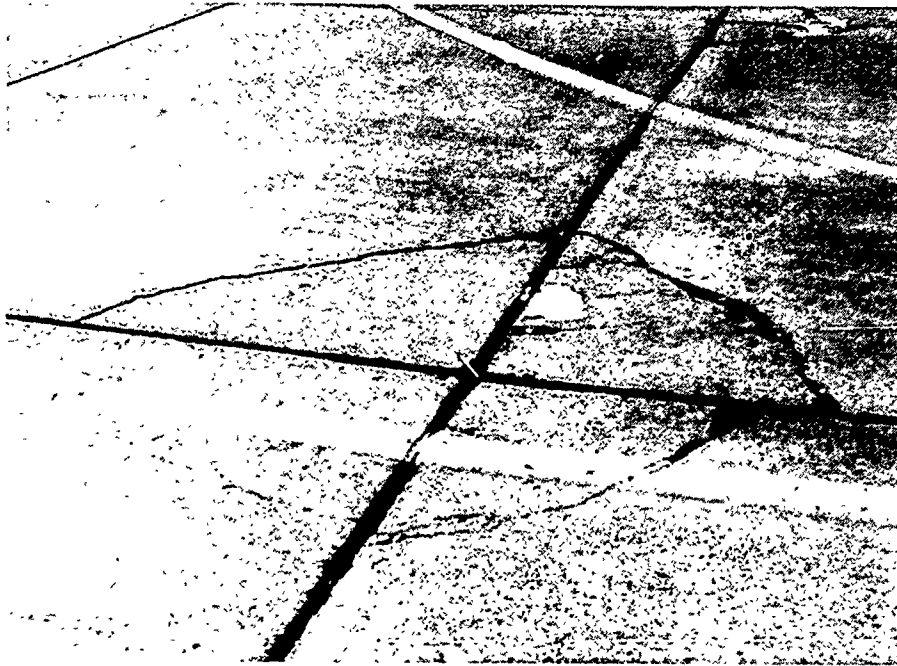


Photo 21. Corner breaks on A31B



Photo 22. Longitudinal/transverse crack on A31B



Photo 23. Spalling corner on A31B



Photo 24. Replaced slabs on T22B

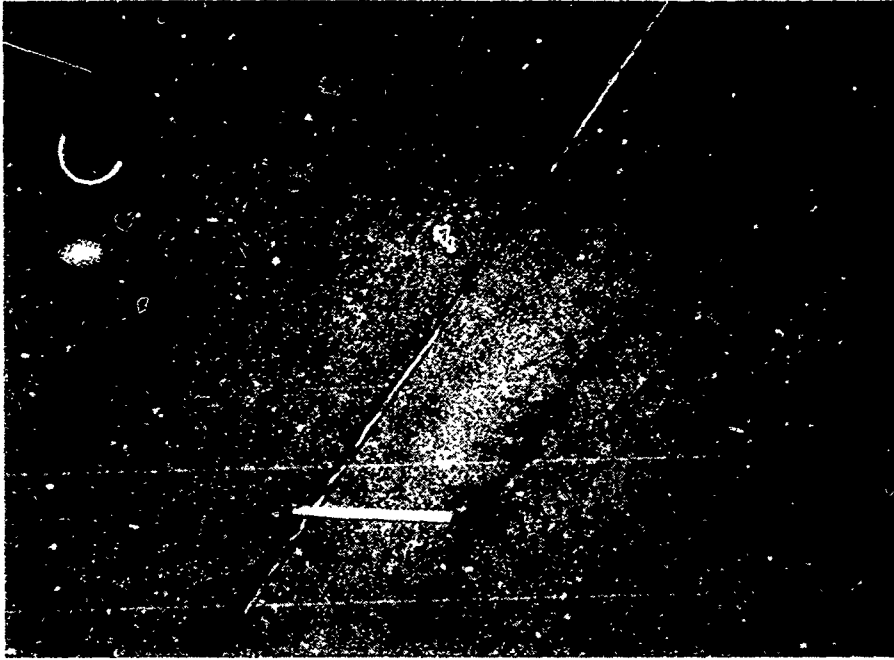


Photo 25. Sawed and sealed joint reflective crack
with longitudinal/transverse crack parallel, T14B



Photo 26. Sawed and sealed reflective joint, T14B

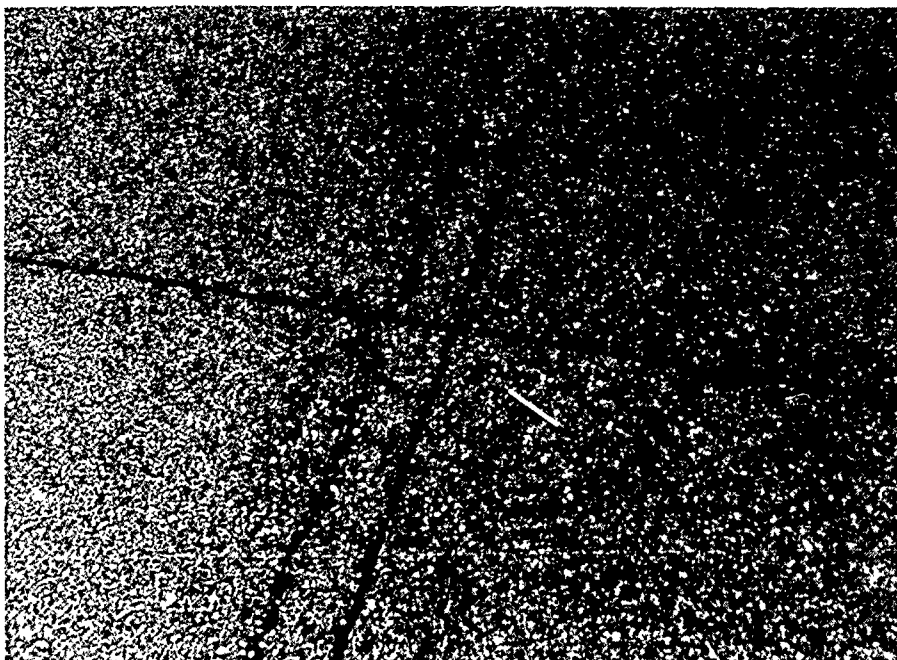


Photo 27. Cracking adjacent to sawed and sealed joint reflective cracking, T14B

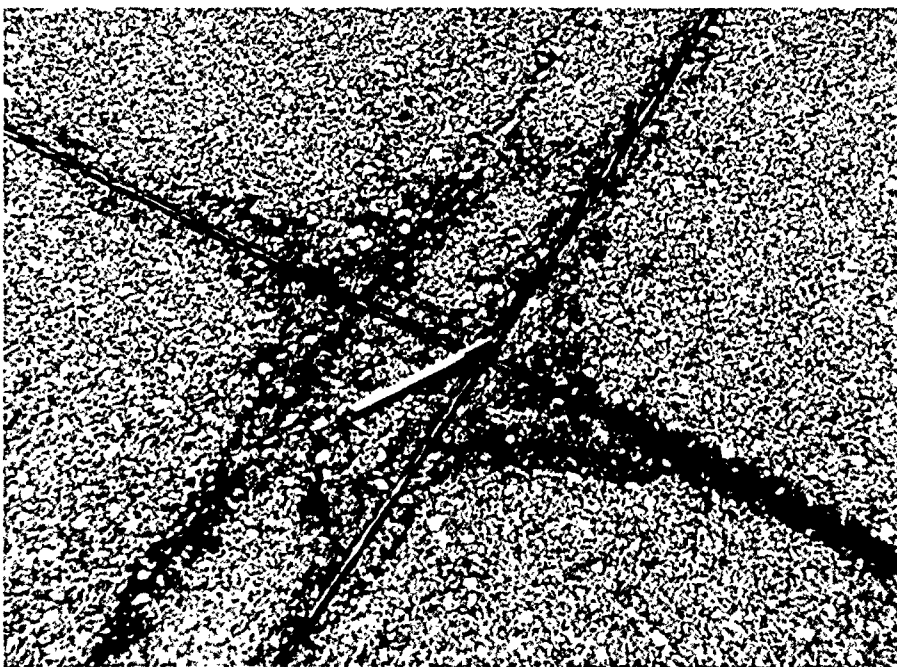


Photo 28. Corner cracking on sawed and sealed joint reflective cracking, T14B

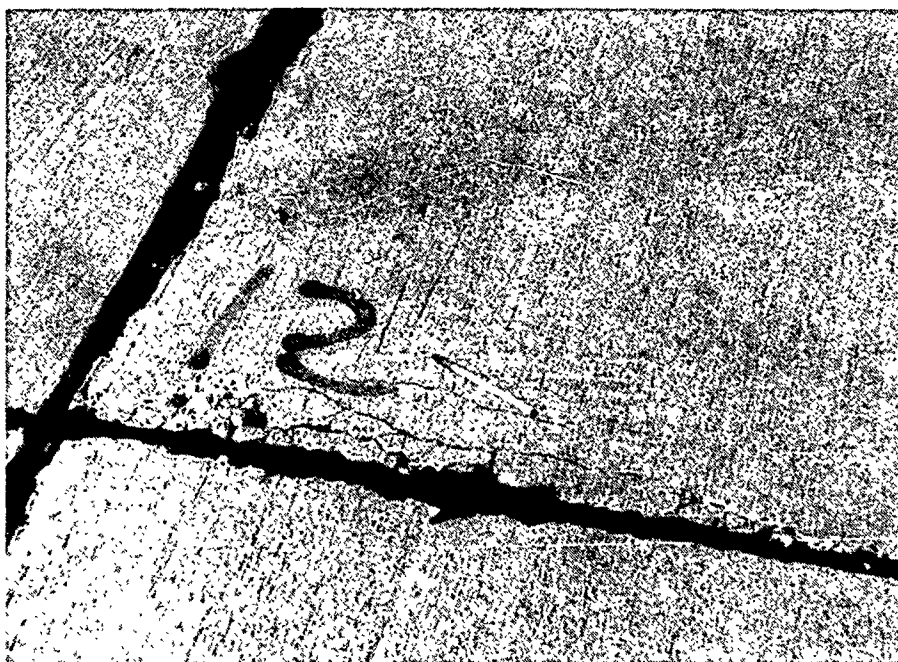


Photo 29. Joint spalling on A10B

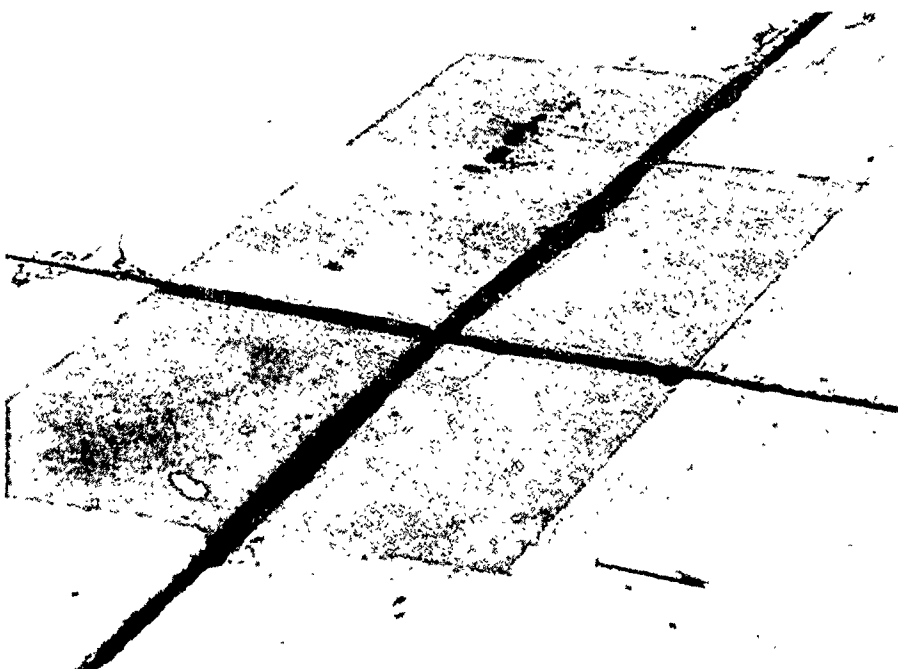


Photo 30. Patching on T4A

APPENDIX A: DATA REPORTS AND ANALYSIS PROGRAMS

BRANCH LISTING

CANNON AFB

| BRANCH NUMBER | BRANCH NAME | NUMBER OF SECTIONS |
|------------------|---------------------------|-----------------------|
| R01A | RUNWAY 04-22 | 3 |
| R02C | RUNWAY 04-22 | 3 |
| R03A | RUNWAY 04-22 | 3 |
| R04A | RUNWAY 13-31 | 3 |
| R05A | RUNWAY 13-31 | 3 |
| R06C | RUNWAY 13-31 | 3 |
| R07C | RUNWAY 13-31 | 3 |
| R08C | RUNWAY 13-31 | 3 |
| R09C | RUNWAY 13-31 | 3 |
| R10A | RUNWAY 13-31 | 3 |
| R11A | RUNWAY 13-31 | 3 |
| O01C | RUNWAY 04-22 OVERRUN | 1 |
| O02C | RUNWAY 04-22 O'RUN BLAST | 1 |
| O03C | RUNWAY 04-22 O'RUN BLAST | 1 |
| O04C | RUNWAY 04-22 OVERRUN | 1 |
| O05C | RUNWAY 13-31 O'RUN BLAST | 1 |
| O06C | RUNWAY 13-31 OVERRUN | 1 |
| O07C | RUNWAY 13-31 O'RUN BLAST | 1 |
| O08C | RUNWAY 13-31 OVERRUN | 1 |
| T03A | TAXIWAYS R AND A | 1 |
| T04A | TAXIWAY A | 1 |
| T05B | TAXIWAY B | 1 |
| T06B | TAXIWAY B | 1 |
| T07C | NORTH CAL PAD ACCESS | 1 |
| T08A | TAXIWAY E | 1 |
| T09A | TAXIWAY R - SOUTH END | 1 |
| T10A | TAXIWAY D | 1 |
| T11A | TAXIWAY D | 1 |
| T12B | TAXIWAY C | 1 |
| T13B | TAXIWAY C | 2 |
| T14B | TAXIWAY C | 1 |
| T15C | TAXIWAY C - FILLET | 1 |
| T16C | TAXIWAY C - PAD ACCESS | 1 |
| T17B | TAXIWAY C | 1 |
| T18B | TAXIWAY C | 1 |
| T19A | TAXIWAY R AT SOUTH CAL | 1 |
| T20A | TAXIWAY R | 1 |
| T22B | SOUTH RAMP TAXIWAY | 1 |
| A01B | SOUTH OPERATIONAL APRON | 1 |
| A02B | MAIN OPERATIONAL APRON | 1 |
| A03B | MAIN OPER APRON-WEST EDGE | 1 |
| A04B | NORTH OPERATIONAL APRON | 1 |
| A05B | MAINTENANCE APRON | 1 |

| | | |
|------|---------------------------|---|
| A06B | SOUTH EDGE-HANGAR ACCESS | 1 |
| A07B | HANGAR ACCESS (BLDG 119) | 1 |
| A08B | HANGAR ACCESS APRONS | 5 |
| A09B | HANGAR ACCESS (BLDG 184) | 2 |
| A10B | MAINTENANCE HANGAR ACCESS | 1 |
| A11B | WARM-UP PAD - TAXIWAY D | 1 |
| A12B | ARM-DISARM PAD - T/W D | 1 |
| A13B | ARM-DISARM ADDITION-T/W D | 1 |
| A14B | ARM-DISARM ADDITION-T/W A | 1 |
| A15B | ARM-DISARM PAD - T/W A | 1 |
| A16B | WARM-UP PAD - TAXIWAY A | 1 |
| A18B | WARM-UP PAD - TAXIWAY C | 1 |
| A26B | NORTH CALIBRATION PAD | 1 |
| A27B | WASHRACK AND ACCESS | 1 |
| A28B | POWER CHECK PAD | 1 |
| A29B | POWER CHECK PAD | 1 |
| A30B | POWER CHECK PAD | 1 |
| A31B | SOUTH RAMP | 1 |
| ROAD | MAIN OPER APRON ROADWAY | 1 |

INV - INVENTORY REPORT

CANNON AFB

INVENTORY SECTION CATEGORY TYPE

| | | SURF TYPE | BRANCH USE | PAVEMENT RANK | AREA (SY) |
|-------|---------------------------|--------------|-------------------|------------------|--------------|
| ----- | | | | | |
| R01A | RUNWAY 04-22 | | | | |
| | SECTION 1 | PCC | RUNWAY | PRIMARY | 5555 |
| | FROM- CENTER 50 FT-10+00 | | | | |
| | TO- 20+00 | | | | |
| | SECTION 2 | PCC | RUNWAY | PRIMARY | 5555 |
| | FROM- NW 50 FT - 10+00 | | | | |
| | TO- 20+00 | | | | |
| | SECTION 3 | PCC | RUNWAY | PRIMARY | 5555 |
| | FROM- SE 50 FT - 10+00 | | | | |
| | TO- 20+00 | | | | |
| | | | TOTAL BRANCH AREA | | 16665 |
| ----- | | | | | |
| R02C | RUNWAY 04-22 | | | | |
| | SECTION 1 | PCC | RUNWAY | PRIMARY | 71111 |
| | FROM- CENTER 80 FT-20+00 | | | | |
| | TO- 100+00 | | | | |
| | SECTION 2 | PCC | RUNWAY | PRIMARY | 31111 |
| | FROM- NW 35 FT - 20+00 | | | | |
| | TO- 100+00 | | | | |
| | SECTION 3 | PCC | RUNWAY | PRIMARY | 31111 |
| | FROM- SE 35 FT - 20+00 | | | | |
| | TO- 100+00 | | | | |
| | | | TOTAL BRANCH AREA | | 133333 |
| ----- | | | | | |
| R03A | RUNWAY 04-22 | | | | |
| | SECTION 1 | PCC | RUNWAY | PRIMARY | 5555 |
| | FROM- CENTER 50 FT-100+00 | | | | |
| | TO- 110+00 | | | | |
| | SECTION 2 | PCC | RUNWAY | PRIMARY | 5555 |
| | FROM- NW 50 FT - 100+00 | | | | |
| | TO- 110+00 | | | | |

| | | | | |
|-------------------------|-----|--------|---------|------|
| SECTION 3 | PCC | RUNWAY | PRIMARY | 5555 |
| FROM- SE 50 FT - 100+00 | | | | |
| TO- 110+00 | | | | |

| | |
|-------------------|-------|
| TOTAL BRANCH AREA | 16665 |
|-------------------|-------|

| | | | | |
|--------------------------|-----|--------|-----------|------|
| R04A RUNWAY 13-31 | | | | |
| SECTION 1 | PCC | RUNWAY | SECONDARY | 1666 |
| FROM- CENTER 50 FT-10+00 | | | | |
| TO- 13+00 | | | | |

| | | | | |
|------------------------|-----|--------|-----------|------|
| SECTION 2 | PCC | RUNWAY | SECONDARY | 1666 |
| FROM- NE 50 FT - 10+00 | | | | |
| TO- 13+00 | | | | |

| | | | | |
|------------------------|-----|--------|-----------|------|
| SECTION 3 | PCC | RUNWAY | SECONDARY | 1666 |
| FROM- SW 50 FT - 10+00 | | | | |
| TO- 13+00 | | | | |

| | |
|-------------------|------|
| TOTAL BRANCH AREA | 4998 |
|-------------------|------|

| | | | | |
|--------------------------|----|--------|---------|------|
| R05A RUNWAY 13-31 | | | | |
| SECTION 1 | AC | RUNWAY | PRIMARY | 3888 |
| FROM- CENTER 50 FT-13+00 | | | | |
| TO- 20+00 | | | | |

| | | | | |
|------------------------|----|--------|-----------|------|
| SECTION 2 | AC | RUNWAY | SECONDARY | 3888 |
| FROM- NE 50 FT - 13+00 | | | | |
| TO- 20+00 | | | | |

| | | | | |
|------------------------|----|--------|-----------|------|
| SECTION 3 | AC | RUNWAY | SECONDARY | 3888 |
| FROM- SW 50 FT - 13+00 | | | | |
| TO- 20+00 | | | | |

| | |
|-------------------|-------|
| TOTAL BRANCH AREA | 11664 |
|-------------------|-------|

| | | | | |
|---------------------------|----|--------|-----------|------|
| R06C RUNWAY 13-31 | | | | |
| SECTION 1 | AC | RUNWAY | SECONDARY | 9777 |
| FROM- CENTER-20+00TO28+35 | | | | |
| TO- PLUS 29+25 TO 38+50 | | | | |

| | | | | |
|---------------------------|----|--------|-----------|------|
| SECTION 2 | AC | RUNWAY | SECONDARY | 9777 |
| FROM- NE 50'-20+00TO28+35 | | | | |
| TO- PLUS 29+25 TO 38+50 | | | | |

| | | | | |
|---------------------------|----|--------|-----------|------|
| SECTION 3 | AC | RUNWAY | SECONDARY | 9777 |
| FROM- SW 50'-20+00TO28+35 | | | | |
| TO- PLUS 29+25 TO 38+50 | | | | |

| | |
|-------------------|-------|
| TOTAL BRANCH AREA | 29331 |
|-------------------|-------|

R07C RUNWAY 13-31
SECTION 1 AC RUNWAY SECONDARY 500
FROM- CENTER 50 FT-28+35
TO- 29+25

SECTION 2 AC RUNWAY SECONDARY 500
FROM- NE 50 FT - 28+35
TO- 29+25

SECTION 3 AC RUNWAY SECONDARY 500
FROM- SW 50 FT - 28+35
TO- 29+25

TOTAL BRANCH AREA 1500

R08C RUNWAY 13-31
SECTION 1 AC RUNWAY SECONDARY 3333
FROM- CENTER 50 FT-40+00
TO- 46+00

SECTION 2 AC RUNWAY SECONDARY 3333
FROM- NE 50 FT - 40+00
TO- 46+00

SECTION 3 AC RUNWAY SECONDARY 3333
FROM- SW 50 FT - 40+00
TO- 46+00

TOTAL BRANCH AREA 9999

R09C RUNWAY 13-31
SECTION 1 AC RUNWAY SECONDARY 20000
FROM- CENTER 50 FT-46+00
TO- 82+00

SECTION 2 AC RUNWAY SECONDARY 20000
FROM- NE 50 FT - 46+00
TO- 82+00

SECTION 3 AC RUNWAY SECONDARY 20000
FROM- SW 50 FT - 46+00
TO- 82+00

TOTAL BRANCH AREA 60000

R10A RUNWAY 13-31
SECTION 1 AC RUNWAY SECONDARY 3888
FROM- CENTER 50 FT-82+00
TO- 89+00

| | | | | |
|-------------------------------|-----|-------------------|-----------|-------|
| SECTION 2 | AC | RUNWAY | SECONDARY | 3888 |
| FROM- NE 50 FT - 82+00 | | | | |
| TO- 89+00 | | | | |
| SECTION 3 | AC | RUNWAY | SECONDARY | 3888 |
| FROM- SW 50 FT - 82+00 | | | | |
| TO- 89+00 | | | | |
| | | TOTAL BRANCH AREA | | 11664 |
| ----- | | | | |
| R11A RUNWAY 13-31 | | | | |
| SECTION 1 | PCC | RUNWAY | SECONDARY | 1666 |
| FROM- CENTER 50 FT-89+00 | | | | |
| TO- 92+00 | | | | |
| SECTION 2 | PCC | RUNWAY | SECONDARY | 1666 |
| FROM- NE 50 FT - 89+00 | | | | |
| TO- 92+00 | | | | |
| SECTION 3 | PCC | RUNWAY | SECONDARY | 1666 |
| FROM- SW 50 FT - 89+00 | | | | |
| TO- 92+00 | | | | |
| | | TOTAL BRANCH AREA | | 4998 |
| ----- | | | | |
| 001C RUNWAY 04-22 OVERRUN | | | | |
| SECTION 1 | ST | RUNWAY | SECONDARY | 14166 |
| FROM- 111+50 | | | | |
| TO- 120+00 | | | | |
| | | TOTAL BRANCH AREA | | 14166 |
| ----- | | | | |
| 002C RUNWAY 04-22 O'RUN BLAST | | | | |
| SECTION 1 | AC | RUNWAY | SECONDARY | 2500 |
| FROM- 110+00 | | | | |
| TO- 111+50 | | | | |
| | | TOTAL BRANCH AREA | | 2500 |
| ----- | | | | |
| 003C RUNWAY 04-22 O'RUN BLAST | | | | |
| SECTION 1 | AC | RUNWAY | SECONDARY | 2500 |
| FROM- 8+50 | | | | |
| TO- 10+00 | | | | |
| | | TOTAL BRANCH AREA | | 2500 |
| ----- | | | | |
| 004C RUNWAY 04-22 OVERRUN | | | | |
| SECTION 1 | ST | RUNWAY | SECONDARY | 14166 |
| FROM- 0+00 | | | | |
| TO- 8+50 | | | | |
| | | TOTAL BRANCH AREA | | 14166 |

| | | | | | |
|-------|--------------------------|-----|---------|-------------------|-------|
| ----- | | | | | |
| 005C | RUNWAY 13-31 O'RUN BLAST | | | | |
| | SECTION 1 | AC | RUNWAY | SECONDARY | 2500 |
| | FROM- 8+50 | | | | |
| | TO- 10+00 | | | | |
| | | | | TOTAL BRANCH AREA | 2500 |
| ----- | | | | | |
| 006C | RUNWAY 13-31 OVERRUN | | | | |
| | SECTION 1 | ST | RUNWAY | SECONDARY | 14166 |
| | FROM- 0+00 | | | | |
| | TO- 8+50 | | | | |
| | | | | TOTAL BRANCH AREA | 14166 |
| ----- | | | | | |
| 007C | RUNWAY 13-31 O'RUN BLAST | | | | |
| | SECTION 1 | AC | RUNWAY | SECONDARY | 2500 |
| | FROM- 92+00 | | | | |
| | TO- 93+50 | | | | |
| | | | | TOTAL BRANCH AREA | 2500 |
| ----- | | | | | |
| 008C | RUNWAY 13-31 OVERRUN | | | | |
| | SECTION 1 | ST | RUNWAY | SECONDARY | 14166 |
| | FROM- 93+50 | | | | |
| | TO- 102+00 | | | | |
| | | | | TOTAL BRANCH AREA | 14166 |
| ----- | | | | | |
| T03A | TAXIWAYS R AND A | | | | |
| | SECTION 1 | PCC | TAXIWAY | PRIMARY | 23333 |
| | FROM- FEATURE T04A | | | | |
| | TO- FEATURE T20A | | | | |
| | | | | TOTAL BRANCH AREA | 23333 |
| ----- | | | | | |
| T04A | TAXIWAY A | | | | |
| | SECTION 1 | PCC | TAXIWAY | PRIMARY | 9483 |
| | FROM- RUNWAY 04-22 | | | | |
| | TO- FEATURE T03A | | | | |
| | | | | TOTAL BRANCH AREA | 9483 |
| ----- | | | | | |
| T05B | TAXIWAY B | | | | |
| | SECTION 1 | PCC | TAXIWAY | SECONDARY | 2961 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 2961 |

| | | | | | |
|-------|--------------------------|-----|-------------------|-----------|-------|
| ----- | | | | | |
| T06B | TAXIWAY B | | | | |
| | SECTION 1 | AC | TAXIWAY | SECONDARY | 7166 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | TOTAL BRANCH AREA | | 7166 |
| ----- | | | | | |
| T07C | NORTH CAL PAD ACCESS | | | | |
| | SECTION 1 | AC | TAXIWAY | SECONDARY | 16041 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | TOTAL BRANCH AREA | | 16041 |
| ----- | | | | | |
| T08A | TAXIWAY E | | | | |
| | SECTION 1 | PCC | TAXIWAY | PRIMARY | 14440 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | TOTAL BRANCH AREA | | 14440 |
| ----- | | | | | |
| T09A | TAXIWAY R - SOUTH END | | | | |
| | SECTION 1 | PCC | TAXIWAY | PRIMARY | 5333 |
| | FROM- 260 FT NE OF 13-31 | | | | |
| | TO- FEATURE T19A | | | | |
| | | | TOTAL BRANCH AREA | | 5333 |
| ----- | | | | | |
| T10A | TAXIWAY D | | | | |
| | SECTION 1 | PCC | TAXIWAY | PRIMARY | 3500 |
| | FROM- 160 FT SW OF 13-31 | | | | |
| | TO- 260 FT NE OF 13-31 | | | | |
| | | | TOTAL BRANCH AREA | | 3500 |
| ----- | | | | | |
| T11A | TAXIWAY D | | | | |
| | SECTION 1 | PCC | TAXIWAY | PRIMARY | 20141 |
| | FROM- EDGE OF RW 04-22 | | | | |
| | TO- 160 FT SW OF 13-31 | | | | |
| | | | TOTAL BRANCH AREA | | 20141 |
| ----- | | | | | |
| T12B | TAXIWAY C | | | | |
| | SECTION 1 | APC | TAXIWAY | SECONDARY | 9439 |
| | FROM- FEATURE T20A | | | | |
| | TO- FEATURE T13B | | | | |
| | | | TOTAL BRANCH AREA | | 9439 |

| | | | | | |
|-------|-------------------------|-----|-------------------|-----------|-------|
| ----- | | | | | |
| T13B | TAXIWAY C | | | | |
| | SECTION 1 | PCC | TAXIWAY | SECONDARY | 2531 |
| | FROM- NW EDGE-RW 04-22 | | | | |
| | TO- 1+75 | | | | |
| | SECTION 2 | PCC | TAXIWAY | SECONDARY | 1727 |
| | FROM- SE EDGE-RW 04-22 | | | | |
| | TO- 1+75 | | | | |
| | | | TOTAL BRANCH AREA | | 4258 |
| ----- | | | | | |
| T14B | TAXIWAY C | | | | |
| | SECTION 1 | APC | TAXIWAY | SECONDARY | 28782 |
| | FROM- FEATURE T13B | | | | |
| | TO- FEATURE T17B | | | | |
| | | | TOTAL BRANCH AREA | | 28782 |
| ----- | | | | | |
| T15C | TAXIWAY C - FILLET | | | | |
| | SECTION 1 | AC | TAXIWAY | SECONDARY | 1313 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | TOTAL BRANCH AREA | | 1313 |
| ----- | | | | | |
| T16C | TAXIWAY C - PAD ACCESS | | | | |
| | SECTION 1 | AC | TAXIWAY | SECONDARY | 2333 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | TOTAL BRANCH AREA | | 2333 |
| ----- | | | | | |
| T17B | TAXIWAY C | | | | |
| | SECTION 1 | AC | TAXIWAY | SECONDARY | 17833 |
| | FROM- FEATURE T18B | | | | |
| | TO- FEATURE T14B | | | | |
| | | | TOTAL BRANCH AREA | | 17833 |
| ----- | | | | | |
| T18B | TAXIWAY C | | | | |
| | SECTION 1 | AC | TAXIWAY | SECONDARY | 75 |
| | FROM- SE END - RW 13-31 | | | | |
| | TO- FEATURE T17B | | | | |
| | | | TOTAL BRANCH AREA | | 075 |
| ----- | | | | | |
| T19A | TAXIWAY R AT SOUTH CAL | | | | |
| | SECTION 1 | PCC | TAXIWAY | PRIMARY | 3166 |
| | FROM- FEATURE T09A | | | | |
| | TO- FEATURE T10A | | | | |
| | | | TOTAL BRANCH AREA | | 3166 |

| | | | | | |
|-------|---------------------------|-----|---------|-------------------|--------|
| ----- | | | | | |
| T20A | TAXIWAY R | | | | |
| | SECTION 1 | PCC | TAXIWAY | PRIMARY | 28000 |
| | FROM- FEATURE T19A | | | | |
| | TO- FEATURE T03A | | | | |
| | | | | TOTAL BRANCH AREA | 28000 |
| ----- | | | | | |
| T22B | SOUTH RAMP TAXIWAY | | | | |
| | SECTION 1 | PCC | TAXIWAY | SECONDARY | 10291 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 10291 |
| ----- | | | | | |
| A01B | SOUTH OPERATIONAL APRON | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 20416 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 20416 |
| ----- | | | | | |
| A02B | MAIN OPERATIONAL APRON | | | | |
| | SECTION 1 | PCC | APRON | PRIMARY | 180000 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 180000 |
| ----- | | | | | |
| A03B | MAIN OPER APRON-WEST EDGE | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 17638 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 17638 |
| ----- | | | | | |
| A04B | NORTH OPERATIONAL APRON | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 71139 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 71139 |
| ----- | | | | | |
| A05B | MAINTENANCE APRON | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 9111 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 9111 |

A06B SOUTH EDGE-HANGAR ACCESS
SECTION 1 PCC APRON SECONDARY 1500
FROM-
TO-
TOTAL BRANCH AREA 1500

A07B HANGAR ACCESS (BLDG 119)
SECTION 1 PCC APRON SECONDARY 4283
FROM-
TO-
TOTAL BRANCH AREA 4283

A08B HANGAR ACCESS APRONS
SECTION 1 PCC APRON SECONDARY 1478
FROM- BUILDING 108
TO-
SECTION 2 PCC APRON SECONDARY 1523
FROM- BUILDING 121
TO-
SECTION 3 PCC APRON SECONDARY 1499
FROM- BUILDING 129
TO-
SECTION 4 PCC APRON SECONDARY 1444
FROM- BUILDING 162
TO-
SECTION 5 PCC APRON SECONDARY 1559
FROM- BUILDING 170
TO-
TOTAL BRANCH AREA 7503

A09B HANGAR ACCESS (BLDG 184)
SECTION 1 PCC APRON SECONDARY 2311
FROM-
TO-
SECTION 2 PCC APRON SECONDARY 2311
FROM-
TO-
TOTAL BRANCH AREA 4622

| | | | | | |
|-------|---------------------------|-----|-------|-------------------|-------|
| ----- | | | | | |
| A10B | MAINTENANCE HANGAR ACCESS | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 11183 |
| | FROM- BUILDINGS 195 196 | | | | |
| | TO- AND 197 | | | | |
| | | | | TOTAL BRANCH AREA | 11183 |
| ----- | | | | | |
| A11B | WARM-UP PAD - TAXIWAY D | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 4728 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 4728 |
| ----- | | | | | |
| A12B | ARM-DISARM PAD - T/W D | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 5867 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 5867 |
| ----- | | | | | |
| A13B | ARM-DISARM ADDITION-T/W D | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 1777 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 1777 |
| ----- | | | | | |
| A14B | ARM-DISARM ADDITION-T/W A | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 1777 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 1777 |
| ----- | | | | | |
| A15B | ARM-DISARM PAD - T/W A | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 5956 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 5956 |
| ----- | | | | | |
| A16B | WARM-UP PAD - TAXIWAY A | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 3831 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 3831 |

| | | | | | |
|-------|-------------------------|-----|-------|-------------------|-------|
| ----- | | | | | |
| A18B | WARM-UP PAD - TAXIWAY C | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 4728 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 4728 |
| ----- | | | | | |
| A26B | NORTH CALIBRATION PAD | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 6600 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 6600 |
| ----- | | | | | |
| A27B | WASHRACK AND ACCESS | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 3719 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 3719 |
| ----- | | | | | |
| A28B | POWER CHECK PAD | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 1833 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 1833 |
| ----- | | | | | |
| A29B | POWER CHECK PAD | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 1556 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 1556 |
| ----- | | | | | |
| A30B | POWER CHECK PAD | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 1844 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 1844 |
| ----- | | | | | |
| A31B | SOUTH RAMP | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 24700 |
| | FROM- | | | | |
| | TO- | | | | |
| | | | | TOTAL BRANCH AREA | 24700 |

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|-------|-------------------------|-----|-------|-----------|-------|
| ----- | | | | | |
| ROAD | MAIN OPER APRON ROADWAY | | | | |
| | SECTION 1 | PCC | APRON | SECONDARY | 10583 |
| | FROM- | | | | |
| | TO- | | | | |

| | | | | | |
|--|--|--|--|-------------------|-------|
| | | | | TOTAL BRANCH AREA | 10583 |
|--|--|--|--|-------------------|-------|

| | | | |
|--|---|-----------|---------|
| | TOTAL AREA OF SELECTED SECTION CATEGORY | PAVEMENTS | 982,263 |
|--|---|-----------|---------|

PCI REPORT

CANNON AFB

| BRANCH NUMBER/NAME | SECTION NUMBER | PAV. RANK | SURF. TYPE | SECTION AREA/SY | INSPECTION DATE | PCI |
|--|-------------------|--------------|---------------|--------------------|--------------------|-----|
| R01A *RUNWAY 04-22 | 1 | PRIMARY | PCC | 5555 | 06/02/89 | 23 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-10+00 [TO]-20+00 | | | | | | |
| R01A *RUNWAY 04-22 | 2 | PRIMARY | PCC | 5555 | 06/02/89 | 63 |
| BRANCH USE- RUNWAY [FROM]- NW 50 FT - 10+00 [TO]-20+00 | | | | | | |
| R01A *RUNWAY 04-22 | 3 | PRIMARY | PCC | 5555 | 06/02/89 | 58 |
| BRANCH USE- RUNWAY [FROM]- SE 50 FT - 10+00 [TO]-20+00 | | | | | | |
| R02C *RUNWAY 04-22 | 1 | PRIMARY | PCC | 71111 | 06/02/89 | 49 |
| BRANCH USE- RUNWAY [FROM]- CENTER 80 FT-20+00 [TO]-100+00 | | | | | | |
| R02C *RUNWAY 04-22 | 2 | PRIMARY | PCC | 31111 | 06/02/89 | 89 |
| BRANCH USE- RUNWAY [FROM]- NW 35 FT - 20+00 [TO]-100+00 | | | | | | |
| R02C *RUNWAY 04-22 | 3 | PRIMARY | PCC | 31111 | 06/02/89 | 87 |
| BRANCH USE- RUNWAY [FROM]- SE 35 FT - 20+00 [TO]-100+00 | | | | | | |
| R03A *RUNWAY 04-22 | 1 | PRIMARY | PCC | 5555 | 06/02/89 | 73 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-100+00 [TO]-110+00 | | | | | | |
| R03A *RUNWAY 04-22 | 2 | PRIMARY | PCC | 5555 | 06/02/89 | 75 |
| BRANCH USE- RUNWAY [FROM]- NW 50 FT - 100+00 [TO]-110+00 | | | | | | |
| R03A *RUNWAY 04-22 | 3 | PRIMARY | PCC | 5555 | 06/02/89 | 88 |
| BRANCH USE- RUNWAY [FROM]- SE 50 FT - 100+00 [TO]-110+00 | | | | | | |
| R04A *RUNWAY 13-31 | 1 | SECONDARY | PCC | 1666 | 06/02/89 | 19 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-10+00 [TO]-13+00 | | | | | | |
| R04A *RUNWAY 13-31 | 2 | SECONDARY | PCC | 1666 | 06/02/89 | 50 |
| BRANCH USE- RUNWAY [FROM]- NE 50 FT - 10+00 [TO]-13+00 | | | | | | |

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|--|---|---------------|------|----------|----|
| R04A *RUNWAY 13-31 | 3 | SECONDARY PCC | 1666 | 06/02/89 | 56 |
| BRANCH USE- RUNWAY [FROM]- SW 50 FT - 10+00 [TO]-13+00 | | | | | |
| R05A *RUNWAY 13-31 | 1 | PRIMARY AC | 3888 | 06/02/89 | 76 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-13+00 [TO]-20+00 | | | | | |
| R05A *RUNWAY 13-31 | 2 | SECONDARY AC | 3888 | 06/02/89 | 88 |
| BRANCH USE- RUNWAY [FROM]- NE 50 FT - 13+00 [TO]-20+00 | | | | | |
| R05A *RUNWAY 13-31 | 3 | SECONDARY AC | 3888 | 06/02/89 | 85 |
| BRANCH USE- RUNWAY [FROM]- SW 50 FT - 13+00 [TO]-20+00 | | | | | |
| R06C *RUNWAY 13-31 | 1 | SECONDARY AC | 9777 | 06/02/89 | 89 |
| BRANCH USE- RUNWAY [FROM]- CENTER-20+00TO23+35 [TO]-PLUS 29+25 TO 38+50 | | | | | |
| R06C *RUNWAY 13-31 | 2 | SECONDARY AC | 9777 | 06/02/89 | 83 |
| BRANCH USE- RUNWAY [FROM]- NE 50' -20+00TO28+35 [TO]-PLUS 29+25 TO 38+50 | | | | | |
| R06C *RUNWAY 13-31 | 3 | SECONDARY AC | 9777 | 06/02/89 | 84 |
| BRANCH USE- RUNWAY [FROM]- SW 50' -20+00TO28+35 [TO]-PLUS 29+25 TO 38+50 | | | | | |
| R07C *RUNWAY 13-31 | 1 | SECONDARY AC | 500 | 06/02/89 | 88 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-28+35 [TO]-29+25 | | | | | |
| R07C *RUNWAY 13-31 | 2 | SECONDARY AC | 500 | 06/02/89 | 84 |
| BRANCH USE- RUNWAY [FROM]- NE 50 FT - 28+35 [TO]-29+25 | | | | | |
| R07C *RUNWAY 13-31 | 3 | SECONDARY AC | 500 | 06/02/89 | 79 |
| BRANCH USE- RUNWAY [FROM]- SW 50 FT - 28+35 [TO]-29+25 | | | | | |
| R08C *RUNWAY 13-31 | 1 | SECONDARY AC | 3333 | 06/02/89 | 96 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-40+00 [TO]-46+00 | | | | | |
| R08C *RUNWAY 13-31 | 2 | SECONDARY AC | 3333 | 06/02/89 | 91 |
| BRANCH USE- RUNWAY [FROM]- NE 50 FT - 40+00 [TO]-46+00 | | | | | |
| R08C *RUNWAY 13-31 | 3 | SECONDARY AC | 3333 | 06/02/89 | 92 |
| BRANCH USE- RUNWAY [FROM]- SW 50 FT - 40+00 [TO]-46+00 | | | | | |

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|--|---|---------------|-------|----------|----|
| R09C *RUNWAY 13-31 | 1 | SECONDARY AC | 20000 | 06/02/89 | 91 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-46+00 [TO]-82+00 | | | | | |
| R09C *RUNWAY 13-31 | 2 | SECONDARY AC | 20000 | 06/02/89 | 90 |
| BRANCH USE- RUNWAY [FROM]- NE 50 FT - 46+00 [TO]-82+00 | | | | | |
| R09C *RUNWAY 13-31 | 3 | SECONDARY AC | 20000 | 06/02/89 | 93 |
| BRANCH USE- RUNWAY [FROM]- SW 50 FT - 46+00 [TO]-82+00 | | | | | |
| R10A *RUNWAY 13-31 | 1 | SECONDARY AC | 3888 | 06/02/89 | 81 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-82+00 [TO]-89+00 | | | | | |
| R10A *RUNWAY 13-31 | 2 | SECONDARY AC | 3888 | 06/02/89 | 83 |
| BRANCH USE- RUNWAY [FROM]- NE 50 FT - 82+00 [TO]-89+00 | | | | | |
| R10A *RUNWAY 13-31 | 3 | SECONDARY AC | 3888 | 06/02/89 | 86 |
| BRANCH USE- RUNWAY [FROM]- SW 50 FT - 82+00 [TO]-89+00 | | | | | |
| R11A *RUNWAY 13-31 | 1 | SECONDARY PCC | 1666 | 06/02/89 | 44 |
| BRANCH USE- RUNWAY [FROM]- CENTER 50 FT-89+00 [TO]-92+00 | | | | | |
| R11A *RUNWAY 13-31 | 2 | SECONDARY PCC | 1666 | 06/02/89 | 49 |
| BRANCH USE- RUNWAY [FROM]- NE 50 FT - 89+00 [TO]-92+00 | | | | | |
| R11A *RUNWAY 13-31 | 3 | SECONDARY PCC | 1666 | 06/02/89 | 76 |
| BRANCH USE- RUNWAY [FROM]- SW 50 FT - 89+00 [TO]-92+00 | | | | | |
| O01C *RUNWAY 04-22 OVERRUN | 1 | SECONDARY ST | 14166 | 06/02/89 | 78 |
| BRANCH USE- RUNWAY [FROM]- 111+50 [TO]-120+00 | | | | | |
| O02C *RUNWAY 04-22 O'RUN BLAST 1 | | SECONDARY AC | 2500 | 06/02/89 | 84 |
| BRANCH USE- RUNWAY [FROM]- 110+00 [TO]-111+50 | | | | | |
| O03C *RUNWAY 04-22 O'RUN BLAST 1 | | SECONDARY AC | 2500 | 06/02/89 | 80 |
| BRANCH USE- RUNWAY [FROM]- 8+50 [TO]-10+00 | | | | | |
| O04C *RUNWAY 04-22 OVERRUN | 1 | SECONDARY ST | 14166 | 06/02/89 | 86 |
| BRANCH USE- RUNWAY [FROM]- 0+00 [TO]-8+50 | | | | | |

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|--|---------------|-------------------------|----------|----|
| 005C *RUNWAY 13-31 O'RUN BLAST 1 | SECONDARY AC | 2500 | 06/02/89 | 64 |
| BRANCH USE- RUNWAY [FROM]- 8+50 | | [TO]-10+00 | | |
| 006C *RUNWAY 13-31 OVERRUN 1 | SECONDARY ST | 14166 | 06/02/89 | 66 |
| BRANCH USE- RUNWAY [FROM]- 0+00 | | [TO]-8+50 | | |
| 007C *RUNWAY 13-31 O'RUN BLAST 1 | SECONDARY AC | 2500 | 06/02/89 | 58 |
| BRANCH USE- RUNWAY [FROM]- 92+00 | | [TO]-93+50 | | |
| 008C *RUNWAY 13-31 OVERRUN 1 | SECONDARY ST | 14166 | 06/02/89 | 92 |
| BRANCH USE- RUNWAY [FROM]- 93+50 | | [TO]-102+00 | | |
| T03A *TAXIWAYS R AND A 1 | PRIMARY PCC | 23333 | 06/02/89 | 59 |
| BRANCH USE- TAXIWAY [FROM]- FEATURE T04A | | [TO]-FEATURE T20A | | |
| T04A *TAXIWAY A 1 | PRIMARY PCC | 9483 | 06/02/89 | 80 |
| BRANCH USE- TAXIWAY [FROM]- | | [TO]- | | |
| T05B *TAXIWAY B 1 | SECONDARY PCC | 2961 | 06/02/89 | 65 |
| BRANCH USE- TAXIWAY [FROM]- RUNWAY 04-22 | | [TO]-FEATURE T03A | | |
| T06B *TAXIWAY B 1 | SECONDARY AC | 7166 | 06/02/89 | 59 |
| BRANCH USE- TAXIWAY [FROM]- | | [TO]- | | |
| T07C *NORTH CAL PAD ACCESS 1 | SECONDARY AC | 16041 | 06/02/89 | 45 |
| BRANCH USE- TAXIWAY [FROM]- | | [TO]- | | |
| T08A *TAXIWAY E 1 | PRIMARY PCC | 14440 | 06/02/89 | 96 |
| BRANCH USE- TAXIWAY [FROM]- | | [TO]- | | |
| T09A *TAXIWAY R - SOUTH END 1 | PRIMARY PCC | 5333 | 06/02/89 | 82 |
| BRANCH USE- TAXIWAY [FROM]- 260 FT NE OF 13-31 | | [TO]-FEATURE T19A | | |
| T10A *TAXIWAY D 1 | PRIMARY PCC | 3500 | 06/02/89 | 53 |
| BRANCH USE- TAXIWAY [FROM]- 160 FT SW OF 13-31 | | [TO]-260 FT NE OF 13-31 | | |
| T11A *TAXIWAY D 1 | PRIMARY PCC | 20141 | 06/02/89 | 78 |
| BRANCH USE- TAXIWAY [FROM]- EDGE OF RW 04-22 | | [TO]-160 FT SW OF 13-31 | | |

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|---|---|---------------|--------|----------|----|
| T12B *TAXIWAY C | 1 | SECONDARY APC | 9439 | 06/02/89 | 59 |
| BRANCH USE- TAXIWAY [FROM]- FEATURE T20A [TO]-FEATURE T13B | | | | | |
| T13B *TAXIWAY C | 1 | SECONDARY PCC | 2531 | 06/02/89 | 85 |
| BRANCH USE- TAXIWAY [FROM]- NW EDGE-RW 04-22 [TO]-1+75 | | | | | |
| T14B *TAXIWAY C | 1 | SECONDARY APC | 28782 | 06/02/89 | 59 |
| BRANCH USE- TAXIWAY [FROM]- FEATURE T13B [TO]-FEATURE T17B | | | | | |
| T15C *TAXIWAY C - FILLET | 1 | SECONDARY AC | 1313 | 06/02/89 | 58 |
| BRANCH USE- TAXIWAY [FROM]- [TO]- | | | | | |
| T16C *TAXIWAY C - PAD ACCESS | 1 | SECONDARY AC | 2333 | 06/02/89 | 77 |
| BRANCH USE- TAXIWAY [FROM]- [TO]- | | | | | |
| T17B *TAXIWAY C | 1 | SECONDARY AC | 17833 | 06/02/89 | 69 |
| BRANCH USE- TAXIWAY [FROM]- FEATURE T18B [TO]-FEATURE T14B | | | | | |
| T18B *TAXIWAY C | 1 | SECONDARY AC | 075 | 06/02/89 | 77 |
| BRANCH USE- TAXIWAY [FROM]- SE END - RW 13-31 [TO]-FEATURE T17B | | | | | |
| T19A *TAXIWAY R AT SOUTH CAL | 1 | PRIMARY PCC | 3166 | 06/02/89 | 62 |
| BRANCH USE- TAXIWAY [FROM]- FEATURE T09A [TO]-FEATURE T10A | | | | | |
| T20A *TAXIWAY R | 1 | PRIMARY PCC | 28000 | 06/02/89 | 64 |
| BRANCH USE- TAXIWAY [FROM]- FEATURE T19A [TO]-FEATURE T03A | | | | | |
| T22B *SOUTH RAMP TAXIWAY | 1 | SECONDARY PCC | 10291 | 06/02/89 | 58 |
| BRANCH USE- TAXIWAY [FROM]- [TO]- | | | | | |
| A01B *SOUTH OPERATIONAL APRON | 1 | SECONDARY PCC | 20416 | 06/02/89 | 83 |
| BRANCH USE- APRON [FROM]- [TO]- | | | | | |
| A02B *MAIN OPERATIONAL APRON | 1 | PRIMARY PCC | 180000 | 06/02/89 | 45 |
| BRANCH USE- APRON [FROM]- [TO]- | | | | | |
| A03B *MAIN OPER APRON-WEST EDGE | 1 | SECONDARY PCC | 17638 | 06/02/89 | 77 |
| BRANCH USE- APRON [FROM]- [TO]- | | | | | |

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|---|---|---------------|-------|----------|----|
| A04B *NORTH OPERATIONAL APRON | 1 | SECONDARY PCC | 71139 | 06/02/89 | 73 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A05B *MAINTENANCE APRON | 1 | SECONDARY PCC | 9111 | 06/02/89 | 75 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A06B *SOUTH EDGE-HANGAR ACCESS | 1 | SECONDARY PCC | 1500 | 06/02/89 | 83 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A07B *HANGAR ACCESS (BLDG 119) | 1 | SECONDARY PCC | 4283 | 06/02/89 | 90 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A09B *HANGAR ACCESS (BLDG 184) | 1 | SECONDARY PCC | 2311 | 06/02/89 | 82 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A10B *MAINTENANCE HANGAR ACCESS | 1 | SECONDARY PCC | 11183 | 06/02/89 | 63 |
| BRANCH USE- APRON [FROM]- BUILDINGS 195 196 | | [TO]-AND 197 | | | |
| A11B *WARM-UP PAD - TAXIWAY D | 1 | SECONDARY PCC | 4728 | 06/02/89 | 77 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A12B *ARM-DISARM PAD - T/W D | 1 | SECONDARY PCC | 5867 | 06/02/89 | 56 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A13B *ARM-DISARM ADDITION-T/W D1 | | SECONDARY PCC | 1777 | 06/02/89 | 68 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A14B *ARM-DISARM ADDITION-T/W A1 | | SECONDARY PCC | 1777 | 06/02/89 | 69 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A15B *ARM-DISARM PAD - T/W A | 1 | SECONDARY PCC | 5956 | 06/02/89 | 74 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A16B *WARM-UP PAD - TAXIWAY A | 1 | SECONDARY PCC | 3831 | 06/02/89 | 82 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |
| A18B *WARM-UP PAD - TAXIWAY C | 1 | SECONDARY PCC | 4728 | 06/02/89 | 69 |
| BRANCH USE- APRON [FROM]- | | [TO]- | | | |

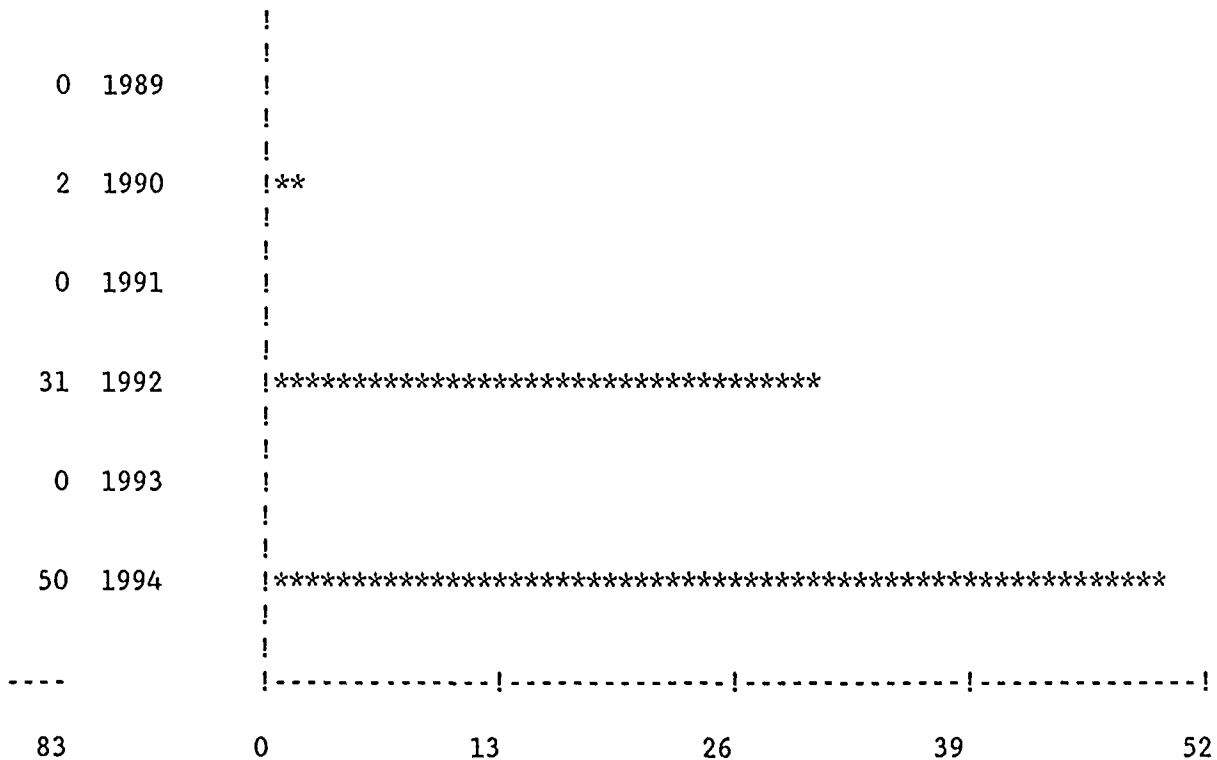
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|-------------------------------|----------|---------------|--------|----------|----|
| A26B *NORTH CALIBRATION PAD | 1 | SECONDARY PCC | 6600 | 06/02/89 | 42 |
| BRANCH USE- APRON | [FROM] - | | [TO] - | | |
| A27B *WASHRACK AND ACCESS | 1 | SECONDARY PCC | 3719 | 06/02/89 | 92 |
| BRANCH USE- APRON | [FROM] - | | [TO] - | | |
| A28B *POWER CHECK PAD | 1 | SECONDARY PCC | 1833 | 06/02/89 | 56 |
| BRANCH USE- APRON | [FROM] - | | [TO] - | | |
| A29B *POWER CHECK PAD | 1 | SECONDARY PCC | 1556 | 06/02/89 | 68 |
| BRANCH USE- APRON | [FROM] - | | [TO] - | | |
| A30B *POWER CHECK PAD | 1 | SECONDARY PCC | 1844 | 06/02/89 | 61 |
| BRANCH USE- APRON | [FROM] - | | [TO] - | | |
| A31B *SOUTH RAMP | 1 | SECONDARY PCC | 24700 | 06/02/89 | 31 |
| BRANCH USE- APRON | [FROM] - | | [TO] - | | |
| ROAD *MAIN OPER APRON ROADWAY | 1 | SECONDARY PCC | 10583 | 06/02/89 | 74 |
| BRANCH USE- APRON | [FROM] - | | [TO] - | | |

INSPECTION SCHEDULE REPORT

| FY TO | NO. OF SECT. | PAVEMENT RANK | | |
|-------|--------------|---------------|-----------|----------|
| INSP. | TO INSP. | PRIMARY | SECONDARY | TERTIARY |
| 1989 | 0 | 0 | 0 | 0 |
| 1990 | 2 | 0 | 2 | 0 |
| 1991 | 0 | 0 | 0 | 0 |
| 1992 | 31 | 6 | 25 | 0 |
| 1993 | 0 | 0 | 0 | 0 |
| 1994 | 50 | 13 | 37 | 0 |

TOTAL NO. OF SECTION: 83
 SECT. NOT NEEDING INSPECTION: 0
 NO. OF MISSING VALUE: 7

NO. FY TO
 SEC. INSP.



NO OF SECTIONS

TOTAL NO. OF SECTION: 83
 SECT. NOT NEEDING INSPECTION: 0
 NO. OF MISSING VALUE: 7

LIST OF CASES IN
INSPECTION SCHEDULE REPORT

| FY TO INSPECT : 1990 | | | | | | NO. OF SECTIONS : 2 | |
|----------------------|---------------|--------------|---------------|-----|-------------|---------------------|----|
| BRANCH NUMBER | BRANCH USE | SECT. NO. | PAVE. RANK | SUT | SEC AREA | FROM | TO |
| T15C | TAXIWAY | 1 | S | AC | 1313 | | |
| A09B | APRON | 1 | S | PCC | 2311 | | |

| FY TO INSPECT : 1992 | | | | | | NO. OF SECTIONS : 31 | |
|----------------------|---------------|--------------|---------------|-----|-------------|----------------------|---------------------|
| BRANCH NUMBER | BRANCH USE | SECT. NO. | PAVE. RANK | SUT | SEC AREA | FROM | TO |
| R01A | RUNWAY | 1 | P | PCC | 5555 | CENTER 50 FT-10+00 | 20+00 |
| R03A | RUNWAY | 1 | P | PCC | 5555 | CENTER 50 FT-100+00 | 110+00 |
| R03A | RUNWAY | 2 | P | PCC | 5555 | NW 50 FT - 100+00 | 110+00 |
| R04A | RUNWAY | 1 | S | PCC | 1666 | CENTER 50 FT-10+00 | 13+00 |
| R05A | RUNWAY | 1 | P | AC | 3888 | CENTER 50 FT-13+00 | 20+00 |
| R05A | RUNWAY | 2 | S | AC | 3888 | NE 50 FT - 13+00 | 20+00 |
| R05A | RUNWAY | 3 | S | AC | 3888 | SW 50 FT - 13+00 | 20+00 |
| R06C | RUNWAY | 1 | S | AC | 9777 | CENTER-20+00TO28+35 | PLUS 29+25 TO 38+50 |
| R06C | RUNWAY | 2 | S | AC | 9777 | NE 50' -20+00TO28+35 | PLUS 29+25 TO 38+50 |
| R06C | RUNWAY | 3 | S | AC | 9777 | SW 50' -20+00TO28+35 | PLUS 29+25 TO 38+50 |
| R07C | RUNWAY | 1 | S | AC | 500 | CENTER 50 FT-28+35 | 29+25 |
| R07C | RUNWAY | 2 | S | AC | 500 | NE 50 FT - 28+35 | 29+25 |
| R07C | RUNWAY | 3 | S | AC | 500 | SW 50 FT - 28+35 | 29+25 |
| R08C | RUNWAY | 2 | S | AC | 3333 | NE 50 FT - 40+00 | 46+00 |
| R09C | RUNWAY | 1 | S | AC | 20000 | CENTER 50 FT-46+00 | 82+00 |
| R09C | RUNWAY | 2 | S | AC | 20000 | NE 50 FT - 46+00 | 82+00 |
| R10A | RUNWAY | 1 | S | AC | 3888 | CENTER 50 FT-82+00 | 89+00 |
| R10A | RUNWAY | 2 | S | AC | 3888 | NE 50 FT - 82+00 | 89+00 |
| R10A | RUNWAY | 3 | S | AC | 3888 | SW 50 FT - 82+00 | 89+00 |
| O05C | RUNWAY | 1 | S | AC | 2500 | 8+50 | 10+00 |
| O06C | RUNWAY | 1 | S | ST | 14166 | 0+00 | 8+50 |
| O07C | RUNWAY | 1 | S | AC | 2500 | 92+00 | 93+50 |
| T08A | TAXIWAY | 1 | P | PCC | 14440 | | |
| T10A | TAXIWAY | 1 | P | PCC | 3500 | 160 FT SW OF 13-31 | 260 FT NE OF 13-31 |
| T12B | TAXIWAY | 1 | S | APC | 9439 | FEATURE T20A | FEATURE T13B |
| T14B | TAXIWAY | 1 | S | APC | 28782 | FEATURE T13B | FEATURE T17B |
| T17B | TAXIWAY | 1 | S | AC | 17833 | FEATURE T18B | FEATURE T14B |
| T18B | TAXIWAY | 1 | S | AC | 75 | SE END - RW 13-31 | FEATURE T17B |
| A01B | APRON | 1 | S | PCC | 20416 | | |
| A10B | APRON | 1 | S | PCC | 11183 | BUILDINGS 195 196 | AND 197 |
| A31B | APRON | 1 | S | PCC | 24700 | | |

| FY TO INSPECT : 1994 | | | | NO. OF SECTIONS : 50 | | | |
|----------------------|---------------|--------------|---------------|----------------------|-------------|--------------------|--------------------|
| BRANCH NUMBER | BRANCH USE | SECT. NO. | PAVE. RANK | SUT | SEC AREA | FROM | TO |
| R01A | RUNWAY | 2 | P | PCC | 5555 | NW 50 FT - 10+00 | 20+00 |
| R01A | RUNWAY | 3 | P | PCC | 5555 | SE 50 FT - 10+00 | 20+00 |
| R02C | RUNWAY | 1 | P | PCC | 71111 | CENTER 80 FT-20+00 | 100+00 |
| R02C | RUNWAY | 2 | P | PCC | 31111 | NW 35 FT - 20+00 | 100+00 |
| R02C | RUNWAY | 3 | P | PCC | 31111 | SE 35 FT - 20+00 | 100+00 |
| R03A | RUNWAY | 3 | P | PCC | 5555 | SE 50 FT - 100+00 | 110+00 |
| R04A | RUNWAY | 2 | S | PCC | 1666 | NE 50 FT - 10+00 | 13+00 |
| R04A | RUNWAY | 3 | S | PCC | 1666 | SW 50 FT - 10+00 | 13+00 |
| R08C | RUNWAY | 1 | S | AC | 3333 | CENTER 50 FT-40+00 | 46+00 |
| R08C | RUNWAY | 3 | S | AC | 3333 | SW 50 FT - 40+00 | 46+00 |
| R09C | RUNWAY | 3 | S | AC | 20000 | SW 50 FT - 46+00 | 82+00 |
| R11A | RUNWAY | 1 | S | PCC | 1666 | CENTER 50 FT-89+00 | 92+00 |
| R11A | RUNWAY | 2 | S | PCC | 1666 | NE 50 FT - 89+00 | 92+00 |
| R11A | RUNWAY | 3 | S | PCC | 1666 | SW 50 FT - 89+00 | 92+00 |
| O01C | RUNWAY | 1 | S | ST | 14166 | 111+50 | 120+00 |
| O02C | RUNWAY | 1 | S | AC | 2500 | 110+00 | 111+50 |
| O03C | RUNWAY | 1 | S | AC | 2500 | 8+50 | 10+00 |
| O04C | RUNWAY | 1 | S | ST | 14166 | 0+00 | 8+50 |
| O08C | RUNWAY | 1 | S | ST | 14166 | 93+50 | 102+00 |
| T03A | TAXIWAY | 1 | P | PCC | 23333 | FEATURE T04A | FEATURE T20A |
| T04A | TAXIWAY | 1 | P | PCC | 9483 | RUNWAY 04-22 | FEATURE T03A |
| T05B | TAXIWAY | 1 | S | PCC | 2961 | | |
| T06B | TAXIWAY | 1 | S | AC | 7166 | | |
| T07C | TAXIWAY | 1 | S | AC | 16041 | | |
| T09A | TAXIWAY | 1 | P | PCC | 5333 | 260 FT NE OF 13-31 | FEATURE T19A |
| T11A | TAXIWAY | 1 | P | PCC | 20141 | EDGE OF RW 04-22 | 160 FT SW OF 13-31 |
| T13B | TAXIWAY | 1 | S | PCC | 2531 | NW EDGE-RW 04-22 | 1+75 |
| T16C | TAXIWAY | 1 | S | AC | 2333 | | |
| T19A | TAXIWAY | 1 | P | PCC | 3166 | FEATURE T09A | FEATURE T10A |
| T20A | TAXIWAY | 1 | P | PCC | 28000 | FEATURE T19A | FEATURE T03A |
| T22B | TAXIWAY | 1 | S | PCC | 10291 | | |
| A02B | APRON | 1 | P | PCC | 180000 | | |
| A03B | APRON | 1 | S | PCC | 17638 | | |
| A04B | APRON | 1 | S | PCC | 71139 | | |
| A05B | APRON | 1 | S | PCC | 9111 | | |
| A06B | APRON | 1 | S | PCC | 1500 | | |
| A07B | APRON | 1 | S | PCC | 4283 | | |
| A11B | APRON | 1 | S | PCC | 4728 | | |
| A12B | APRON | 1 | S | PCC | 5867 | | |
| A13B | APRON | 1 | S | PCC | 1777 | | |
| A14B | APRON | 1 | S | PCC | 1777 | | |
| A15B | APRON | 1 | S | PCC | 5956 | | |
| A16B | APRON | 1 | S | PCC | 3831 | | |
| A18B | APRON | 1 | S | PCC | 4728 | | |
| A26B | APRON | 1 | S | PCC | 6600 | | |
| A27B | APRON | 1 | S | PCC | 3719 | | |
| A28B | APRON | 1 | S | PCC | 1833 | | |
| A29B | APRON | 1 | S | PCC | 1556 | | |

| | | | | | |
|------|-------|---|---|-----|-------|
| A30B | APRON | 1 | S | PCC | 1844 |
| ROAD | APRON | 1 | S | PCC | 10583 |

TOTAL NO. OF SECTION: 83
 SECT. NOT NEEDING INSPECTION: 0
 NO. OF MISSING VALUE: 7
 MISSING BR NO. SEC. NO.

| | |
|------|---|
| T13B | 2 |
| A08B | 5 |
| A08B | 4 |
| A08B | 3 |
| A08B | 2 |
| A08B | 1 |
| A09B | 2 |

MINIMUM PCI TABLE

| | | | |
|---------|---|---|---|
| | P | S | T |
| RUNWAY | 0 | 0 | 0 |
| APRON | 0 | 0 | 0 |
| TAXIWAY | 0 | 0 | 0 |

| | |
|--------------|-------------|
| | RATE LIMIT |
| RATE(PTS/YR) | YRS TO INSP |
| GT 10 | 1 |
| 6 - 10 | 1 |
| 2 - 5 | 3 |
| LT 2 | 5 |

SAMPCUR - CURRENT INSPECTION RESULTS

REPORT DATE- 07/20/89

PAVEMENT INSPECTION

AGENCY NUMBER = 1 CANNON AFB

BRANCH NAME - RUNWAY 04-22 SLAB LENGTH - 25.0 LF
BRANCH NUMBER - R01A SLAB WIDTH - 25.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 80

INSPECTION DATE - 06/02/89 PCI= 23 RATING= VERY POOR
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 4
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 23

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 10 SLABS | 50.00 | 18.0 |
| 67 LG PATCH/UTIL | MEDIUM | 3 SLABS | 15.00 | 20.3 |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 |
| 63 LINEAR CR | LOW | 4 SLABS | 20.00 | 13.7 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 15.00 | 24.0 |
| 66 SMALL PATCH | HIGH | 3 SLABS | 15.00 | 14.2 |
| 66 SMALL PATCH | MEDIUM | 3 SLABS | 15.00 | 7.6 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 4 SLABS | 5.00 | 13.2 |
| 74 JOINT SPALLING | MEDIUM | 4 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 80 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 40 SLABS | 50.00 | 18.0 |
| 67 LG PATCH/UTIL | MEDIUM | 12 SLABS | 15.00 | 20.3 |
| 63 LINEAR CR | HIGH | 4 SLABS | 5.00 | 16.2 |
| 63 LINEAR CR | LOW | 16 SLABS | 20.00 | 13.7 |
| 63 LINEAR CR | MEDIUM | 12 SLABS | 15.00 | 24.0 |
| 66 SMALL PATCH | HIGH | 12 SLABS | 15.00 | 14.2 |
| 66 SMALL PATCH | MEDIUM | 12 SLABS | 15.00 | 7.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 38.86 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 5.05 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = 56.09 PERCENT DEDUCT VALUES.

 BRANCH NAME - RUNWAY 04-22 SLAB LENGTH - 25.0 LF
 BRANCH NUMBER - R01A SLAB WIDTH - 25.0 LF
 SECTION NUMBER - 2 NUMBER OF SLABS - 80

INSPECTION DATE - 06/02/89 PCI= 63 RATING= GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 4
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 63

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | MEDIUM | 2 SLABS | 10.00 | 16.8 |
| 63 LINEAR CR | LOW | 10 SLABS | 50.00 | 20.2 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 10.00 | 1.5 |
| 66 SMALL PATCH | LOW | 7 SLABS | 35.00 | 5.0 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 80 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | MEDIUM | 8 SLABS | 10.00 | 16.8 |
| 63 LINEAR CR | LOW | 40 SLABS | 50.00 | 20.2 |
| 73 SHRINKAGE CR | N/A | 8 SLABS | 10.00 | 1.5 |
| 66 SMALL PATCH | LOW | 28 SLABS | 35.00 | 5.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***
 LOAD RELATED DISTRESSES = 40.00 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 13.86 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = 46.14 PERCENT DEDUCT VALUES.

 BRANCH NAME - RUNWAY 04-22 SLAB LENGTH - 25.0 LF
 BRANCH NUMBER - R01A SLAB WIDTH - 25.0 LF
 SECTION NUMBER - 3 NUMBER OF SLABS - 80

INSPECTION DATE - 06/02/89 PCI= 58 RATING= GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 4
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 58

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |
| 63 LINEAR CR | LOW | 4 SLABS | 20.00 | 13.7 |
| 72 SHATTERED SLAB | HIGH | 1 SLABS | 5.00 | 30.0 |
| 73 SHRINKAGE CR | N/A | 4 SLABS | 20.00 | 2.9 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 67 LG PATCH/UTIL | LOW | 4 SLABS | 5.00 | 3.1 |
| 63 LINEAR CR | LOW | 16 SLABS | 20.00 | 13.7 |
| 72 SHATTERED SLAB | HIGH | 4 SLABS | 5.00 | 30.0 |
| 73 SHRINKAGE CR | N/A | 16 SLABS | 20.00 | 2.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 87.93 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 12.07 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|-------------------|---------|
| BRANCH NAME - | RUNWAY 04-22 | SLAB LENGTH - | 20.0 LF |
| BRANCH NUMBER - | R02C | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 1600 |

| | | | | | |
|--|----------|-----------|------------|----------|------|
| INSPECTION DATE - | 06/02/89 | PCI= | 49 | RATING= | FAIR |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 80 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 8 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 11 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 7.2 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-12 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 43 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |
| 67 LG PATCH/UTIL | LOW | 8 SLABS | 40.00 | 16.0 |
| 67 LG PATCH/UTIL | MEDIUM | 5 SLABS | 25.00 | 26.5 |
| 70 SCALING/CRAZING | LOW | 16 SLABS | 80.00 | 15.2 |
| 66 SMALL PATCH | HIGH | 1 SLABS | 5.00 | 6.3 |
| 66 SMALL PATCH | LOW | 5 SLABS | 25.00 | 3.2 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 38 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |
| 67 LG PATCH/UTIL | HIGH | 1 SLABS | 5.00 | 17.7 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |
| 67 LG PATCH/UTIL | MEDIUM | 12 SLABS | 60.00 | 41.5 |
| 70 SCALING/CRAZING | LOW | 16 SLABS | 80.00 | 15.2 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |
| 66 SMALL PATCH | MEDIUM | 3 SLABS | 15.00 | 7.6 |

SAMPLE UNIT-22 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 53

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |
| 67 LG PATCH/UTIL | LOW | 2 SLABS | 10.00 | 6.0 |
| 67 LG PATCH/UTIL | MEDIUM | 15 SLABS | 75.00 | 45.5 |
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |

SAMPLE UNIT-32 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 54

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |
| 67 LG PATCH/UTIL | LOW | 6 SLABS | 30.00 | 13.3 |
| 67 LG PATCH/UTIL | MEDIUM | 6 SLABS | 30.00 | 29.0 |
| 70 SCALING/CRAZING | LOW | 12 SLABS | 60.00 | 13.5 |
| 66 SMALL PATCH | LOW | 11 SLABS | 55.00 | 7.6 |

SAMPLE UNIT-42 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 50

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |
| 67 LG PATCH/UTIL | MEDIUM | 15 SLABS | 75.00 | 45.5 |
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |

SAMPLE UNIT-52 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 61

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 67 LG PATCH/UTIL | LOW | 2 SLABS | 10.00 | 6.0 |
| 67 LG PATCH/UTIL | MEDIUM | 6 SLABS | 30.00 | 29.0 |
| 66 SMALL PATCH | LOW | 11 SLABS | 55.00 | 7.6 |

SAMPLE UNIT-62 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 51

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 67 LG PATCH/UTIL | MEDIUM | 15 SLABS | 75.00 | 45.5 |
| 66 SMALL PATCH | LOW | 5 SLABS | 25.00 | 3.2 |

SAMPLE UNIT-72 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 44

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |
| 67 LG PATCH/UTIL | MEDIUM | 15 SLABS | 75.00 | 45.5 |
| 66 SMALL PATCH | LOW | 5 SLABS | 25.00 | 3.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 10 SLABS | 0.62 | 0.7 |

| | | | | |
|--------------------|--------|------------|-------|------|
| 74 JOINT SPALLING | HIGH | 20 SLABS | 1.25 | 4.0 |
| 74 JOINT SPALLING | LOW | 10 SLABS | 0.62 | 0.4 |
| 65 JT SEAL DAMAGE | LOW | 1200 SLABS | 75.00 | 2.0 |
| 67 LG PATCH/UTIL | HIGH | 10 SLABS | 0.62 | 2.4 |
| 67 LG PATCH/UTIL | LOW | 200 SLABS | 12.50 | 7.0 |
| 67 LG PATCH/UTIL | MEDIUM | 890 SLABS | 55.62 | 40.1 |
| 70 SCALING/CRAZING | LOW | 440 SLABS | 27.50 | 8.8 |
| 73 SHRINKAGE CR | N/A | 10 SLABS | 0.62 | 0.2 |
| 66 SMALL PATCH | HIGH | 10 SLABS | 0.62 | 1.0 |
| 66 SMALL PATCH | LOW | 470 SLABS | 29.37 | 3.8 |
| 66 SMALL PATCH | MEDIUM | 30 SLABS | 1.87 | 1.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 2.80 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 97.20 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|-------------------|---------|
| BRANCH NAME - | RUNWAY 04-22 | SLAB LENGTH - | 17.5 LF |
| BRANCH NUMBER - | R02C | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 2 | NUMBER OF SLABS - | 800 |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 89 | RATING= | EXCELLENT |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 40 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 4 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 26 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 12.7 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-14 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 90 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|----------------|----------|----------|-------------|--------------|
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 66 SMALL PATCH | LOW | 7 SLABS | 35.00 | 5.0 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-24 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 96 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|----------------|----------|----------|-------------|--------------|
| 66 SMALL PATCH | LOW | 6 SLABS | 30.00 | 4.0 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-34 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 98 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|----------------|----------|----------|-------------|--------------|
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-4 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 70 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 75 CORNER SPALLING | MEDIUM | 3 SLABS | 15.00 | 9.7 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

```

BRANCH NAME -    RUNWAY 04-22          SLAB LENGTH - 17.5 LF
BRANCH NUMBER -    R02C                SLAB WIDTH - 20.0 LF
SECTION NUMBER - 3                     NUMBER OF SLABS - 800

```

| | | |
|--|------------|-------------------|
| INSPECTION DATE - 06/02/89 | PCI= 87 | RATING= EXCELLENT |
| CONDITION- RIDING- SAFETY- DRAINAGE- | SHOULDERS- | OVERALL- |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 40 |
| NUMBER OF SAMPLES SURVEYED= | | 3 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | 22 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | 7.9 |

SAMPLE UNIT-17 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 79

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |
| 67 LG PATCH/UTIL | LOW | 3 SLABS | 15.00 | 8.0 |
| 70 SCALING/CRAZING | LOW | 10 SLABS | 50.00 | 12.2 |
| 66 SMALL PATCH | LOW | 10 SLABS | 50.00 | 7.0 |

SAMPLE UNIT-27 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 87

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 2 SLABS | 10.00 | 9.0 |
| 66 SMALL PATCH | LOW | 11 SLABS | 55.00 | 7.6 |

SAMPLE UNIT-7 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 95

A33

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 27 SLABS | 3.37 | 3.6 |
| 65 JT SEAL DAMAGE | LOW | 267 SLABS | 33.37 | 2.0 |
| 67 LG PATCH/UTIL | LOW | 53 SLABS | 6.62 | 4.1 |
| 70 SCALING/CRAZING | LOW | 133 SLABS | 16.62 | 6.0 |
| 66 SMALL PATCH | LOW | 320 SLABS | 40.00 | 5.7 |

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 9.35 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 90.65 PERCENT DEDUCT VALUES. |

| | | |
|--|------------|-------------------|
| INSPECTION DATE - 06/02/89 | PCI= 73 | RATING= VERY GOOD |
| CONDITION- RIDING- SAFETY- DRAINAGE- | SHOULDERS- | OVERALL- |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 4 |
| NUMBER OF SAMPLES SURVEYED= | | 1 |
| RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. | | |

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 74 JOINT SPALLING | MEDIUM | 3 SLABS | 15.00 | 10.9 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 15.00 | 2.1 |

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 4 SLABS | 5.00 | 1.9 |
| 74 JOINT SPALLING | LOW | 4 SLABS | 5.00 | 2.2 |
| 74 JOINT SPALLING | MEDIUM | 12 SLABS | 15.00 | 10.9 |
| 63 LINEAR CR | LOW | 4 SLABS | 5.00 | 4.9 |
| 63 LINEAR CR | MEDIUM | 4 SLABS | 5.00 | 11.6 |
| 73 SHRINKAGE CR | N/A | 12 SLABS | 15.00 | 2.1 |

INSPECTION DATE - 06/02/89 PCI= 75 RATING= VERY GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 4
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 67 LG PATCH/UTIL | LOW | 20 SLABS | 100.00 | 22.3 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 |

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 4 SLABS | 5.00 | 2.2 |
| 67 LG PATCH/UTIL | LOW | 80 SLABS | 100.00 | 22.3 |
| 66 SMALL PATCH | LOW | 4 SLABS | 5.00 | 0.6 |

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |

| | | |
|--|------------|-------------------|
| INSPECTION DATE - 06/02/89 | PCI= 88 | RATING= EXCELLENT |
| CONDITION- RIDING- SAFETY- DRAINAGE- | SHOULDERS- | OVERALL- |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 4 |
| NUMBER OF SAMPLES SURVEYED= | | 1 |
| RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. | | |

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|----------------|----------|----------|-------------|--------------|
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 |

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|----------------|----------|----------|-------------|--------------|
| 63 LINEAR CR | MEDIUM | 4 SLABS | 5.00 | 11.6 |
| 66 SMALL PATCH | LOW | 4 SLABS | 5.00 | 0.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 95.08 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = 4.92 PERCENT DEDUCT VALUES.

 BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF
 BRANCH NUMBER - R04A SLAB WIDTH - 12.5 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 96

INSPECTION DATE - 06/02/89 PCI= 19 RATING= VERY POOR
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 5
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 15

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 2 SLABS | 10.00 | 8.0 |
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 75 CORNER SPALLING | LOW | 3 SLABS | 15.00 | 5.5 |
| 64 DURABILITY CR | HIGH | 1 SLABS | 5.00 | 8.3 |
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 74 JOINT SPALLING | MEDIUM | 2 SLABS | 10.00 | 8.0 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | MEDIUM | 5 SLABS | 25.00 | 26.5 |
| 63 LINEAR CR | HIGH | 3 SLABS | 15.00 | 33.1 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 |
| 72 SHATTERED SLAB | HIGH | 1 SLABS | 5.00 | 30.0 |
| 73 SHRINKAGE CR | N/A | 13 SLABS | 65.00 | 10.0 |

 SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 16 SLABS SAMPLE PCI- 22

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | MEDIUM | 1 SLABS | 6.25 | 10.3 |
| 75 CORNER SPALLING | HIGH | 1 SLABS | 6.25 | 6.1 |
| 75 CORNER SPALLING | LOW | 2 SLABS | 12.50 | 4.7 |
| 75 CORNER SPALLING | MEDIUM | 2 SLABS | 12.50 | 8.5 |
| 74 JOINT SPALLING | HIGH | 2 SLABS | 12.50 | 23.4 |
| 74 JOINT SPALLING | LOW | 3 SLABS | 18.75 | 5.6 |
| 74 JOINT SPALLING | MEDIUM | 4 SLABS | 25.00 | 16.0 |
| 65 JT SEAL DAMAGE | HIGH | 16 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | HIGH | 1 SLABS | 6.25 | 21.0 |
| 67 LG PATCH/UTIL | LOW | 4 SLABS | 25.00 | 12.0 |
| 63 LINEAR CR | LOW | 2 SLABS | 12.50 | 10.0 |
| 73 SHRINKAGE CR | N/A | 7 SLABS | 43.75 | 6.3 |
| 66 SMALL PATCH | LOW | 3 SLABS | 18.75 | 2.0 |
| 66 SMALL PATCH | MEDIUM | 1 SLABS | 6.25 | 3.3 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 5 SLABS | 5.20 | 4.2 |
| 62 CORNER BR | MEDIUM | 3 SLABS | 3.12 | 5.1 |
| 75 CORNER SPALLING | HIGH | 5 SLABS | 5.20 | 5.2 |
| 75 CORNER SPALLING | LOW | 13 SLABS | 13.54 | 5.0 |
| 75 CORNER SPALLING | MEDIUM | 5 SLABS | 5.20 | 3.5 |
| 64 DURABILITY CR | HIGH | 3 SLABS | 3.12 | 5.5 |
| 74 JOINT SPALLING | HIGH | 5 SLABS | 5.20 | 13.6 |
| 74 JOINT SPALLING | LOW | 13 SLABS | 13.54 | 4.4 |
| 74 JOINT SPALLING | MEDIUM | 16 SLABS | 16.66 | 11.7 |
| 65 JT SEAL DAMAGE | HIGH | 96 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | HIGH | 3 SLABS | 3.12 | 11.9 |
| 67 LG PATCH/UTIL | LOW | 11 SLABS | 11.45 | 6.5 |
| 67 LG PATCH/UTIL | MEDIUM | 13 SLABS | 13.54 | 19.3 |
| 63 LINEAR CR | HIGH | 8 SLABS | 8.33 | 22.9 |
| 63 LINEAR CR | LOW | 5 SLABS | 5.20 | 5.0 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 3.12 | 7.8 |
| 72 SHATTERED SLAB | HIGH | 3 SLABS | 3.12 | 16.2 |
| 73 SHRINKAGE CR | N/A | 53 SLABS | 55.20 | 8.0 |
| 66 SMALL PATCH | LOW | 8 SLABS | 8.33 | 1.0 |
| 66 SMALL PATCH | MEDIUM | 3 SLABS | 3.12 | 1.7 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 35.89 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 10.26 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 53.84 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|-------------------|---------|
| BRANCH NAME - | RUNWAY 13-31 | SLAB LENGTH - | 12.5 LF |
| BRANCH NUMBER - | R04A | SLAB WIDTH - | 12.5 LF |
| SECTION NUMBER - | 2 | NUMBER OF SLABS - | 96 |

| | | | | | |
|-------------------------------------|----------|-----------|------------|----------|------|
| INSPECTION DATE - | 06/02/89 | PCI= | 50 | RATING= | FAIR |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 5 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 2 |

RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 44 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 4 SLABS | 20.00 | 7.0 |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 74 JOINT SPALLING | LOW | 8 SLABS | 40.00 | 9.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |
| 63 LINEAR CR | HIGH | 2 SLABS | 10.00 | 26.0 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 10.00 | 1.5 |

| | | | | |
|------------------------|----------|--------------|-------------|----------------|
| 66 SMALL PATCH | LOW | 3 SLABS | 15.00 | 1.6 |
| ----- | | | | |
| SAMPLE UNIT-4 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 56 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 8 SLABS | 40.00 | 16.0 |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 |
| 73 SHRINKAGE CR | N/A | 11 SLABS | 55.00 | 8.0 |
| 66 SMALL PATCH | HIGH | 1 SLABS | 5.00 | 6.3 |
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| | | | | |
|--------------------|----------|----------|-------------|--------------|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | HIGH | 2 SLABS | 2.08 | 2.4 |
| 75 CORNER SPALLING | LOW | 10 SLABS | 10.41 | 3.8 |
| 74 JOINT SPALLING | HIGH | 2 SLABS | 2.08 | 6.7 |
| 74 JOINT SPALLING | LOW | 19 SLABS | 19.79 | 5.8 |
| 65 JT SEAL DAMAGE | HIGH | 96 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 22 SLABS | 22.91 | 11.1 |
| 63 LINEAR CR | HIGH | 7 SLABS | 7.29 | 20.9 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 2.08 | 5.2 |
| 73 SHRINKAGE CR | N/A | 31 SLABS | 32.29 | 4.5 |
| 66 SMALL PATCH | HIGH | 2 SLABS | 2.08 | 3.5 |
| 66 SMALL PATCH | LOW | 17 SLABS | 17.70 | 1.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 33.55 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 15.42 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES.

 BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF
 BRANCH NUMBER - R04A SLAB WIDTH - 12.5 LF
 SECTION NUMBER - 3 NUMBER OF SLABS - 96

INSPECTION DATE - 06/02/89 PCI= 56 RATING= GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 5
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

| | | | | |
|------------------------|----------|--------------|-------------|----------------|
| SAMPLE UNIT-1 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 66 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 5.00 | 3.3 |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |

| | | | | |
|------------------------|----------|-----------------------|-------------|----------------|
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 |
| 66 SMALL PATCH | MEDIUM | 1 SLABS | 5.00 | 2.7 |
| ----- | | | | |
| SAMPLE UNIT-4 (RANDOM) | | SAMPLE SIZE- 20 SLABS | | SAMPLE PCI- 46 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 5.00 | 3.3 |
| 74 JOINT SPALLING | LOW | 3 SLABS | 15.00 | 4.7 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |
| 72 SHATTERED SLAB | HIGH | 1 SLABS | 5.00 | 30.0 |
| 73 SHRINKAGE CR | N/A | 6 SLABS | 30.00 | 4.2 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| | | | | |
|--------------------|----------|----------|-------------|--------------|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | HIGH | 2 SLABS | 2.08 | 2.4 |
| 75 CORNER SPALLING | MEDIUM | 5 SLABS | 5.20 | 3.5 |
| 74 JOINT SPALLING | HIGH | 2 SLABS | 2.08 | 6.7 |
| 74 JOINT SPALLING | LOW | 12 SLABS | 12.50 | 4.1 |
| 74 JOINT SPALLING | MEDIUM | 2 SLABS | 2.08 | 2.1 |
| 65 JT SEAL DAMAGE | HIGH | 96 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 2 SLABS | 2.08 | 1.5 |
| 63 LINEAR CR | LOW | 7 SLABS | 7.29 | 6.6 |
| 72 SHATTERED SLAB | HIGH | 2 SLABS | 2.08 | 2.9 |
| 73 SHRINKAGE CR | N/A | 14 SLABS | 14.58 | 2.0 |
| 66 SMALL PATCH | LOW | 7 SLABS | 7.29 | 0.9 |
| 66 SMALL PATCH | MEDIUM | 2 SLABS | 2.08 | 1.1 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 20.74 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 26.20 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = 53.06 PERCENT DEDUCT VALUES.

BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 700 LF
BRANCH NUMBER - R05A SECTION WIDTH - 50 LF
SECTION NUMBER - 1 SECTION AREA - 3888 SY

INSPECTION DATE - 06/02/89 PCI= 76 RATING= VERY GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 7
NUMBER OF SAMPLES SURVEYED= 2
RECOMMENDED SAMPLES TO BE SURVEYED= 7
STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 10.6

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 68

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 160 SF | 3.20 | 32.0 |
| 48 LONG/TRANS CR | LOW | 55 LF | 1.10 | 5.7 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-7 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 83 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 30 SF | 0.60 | 15.7 |
| 48 LONG/TRANS CR | LOW | 243 LF | 4.86 | 13.7 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 665 SF | 1.90 | 26.5 |
| 48 LONG/TRANS CR | LOW | 1043 LF | 2.98 | 9.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 72.80 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 27.20 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|------------------|---------|
| BRANCH NAME - | RUNWAY 13-31 | SECTION LENGTH - | 700 LF |
| BRANCH NUMBER - | R05A | SECTION WIDTH - | 50 LF |
| SECTION NUMBER - | 2 | SECTION AREA - | 3888 SY |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 88 | RATING= | EXCELLENT |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 7 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 2 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 5 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 0.7 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 87 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 230 LF | 4.60 | 13.1 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-5 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 88 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 215 LF | 4.30 | 12.5 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 1558 LF | 4.45 | 12.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | | | |
|-------------------------|----------|----------------------|-------------|----------------|--|
| SAMPLE UNIT-17 (RANDOM) | | SAMPLE SIZE- 5000 SF | | SAMPLE PCI- 85 | |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 41 ALLIGATOR CR | LOW | 20 SF | 0.40 | 12.5 | |
| 48 LONG/TRANS CR | LOW | 200 LF | 4.00 | 11.9 | |
| 50 PATCHING | LOW | 10 SF | 0.20 | 2.0 | |

| | | | | | |
|------------------------|----------|----------------------|-------------|----------------|--|
| SAMPLE UNIT-5 (RANDOM) | | SAMPLE SIZE- 5000 SF | | SAMPLE PCI- 91 | |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 48 LONG/TRANS CR | LOW | 120 LF | 2.40 | 8.8 | |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| | | | | | |
|------------------|----------|----------|-------------|--------------|--|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 41 ALLIGATOR CR | LOW | 117 SF | 0.13 | 7.5 | |
| 48 LONG/TRANS CR | LOW | 2759 LF | 3.13 | 10.2 | |
| 50 PATCHING | LOW | 39 SF | 0.06 | 1.2 | |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 39.68 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 60.32 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|------------------|---------|
| BRANCH NAME - | RUNWAY 13-31 | SECTION LENGTH - | 1760 LF |
| BRANCH NUMBER - | R06C | SECTION WIDTH - | 50 LF |
| SECTION NUMBER - | 2 | SECTION AREA - | 9777 SY |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 83 | RATING= | VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | 18 | |
| NUMBER OF SAMPLES SURVEYED= | | | | 3 | |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | 5 | |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | 1.0 | |

| | | | | | |
|-------------------------|----------|----------------------|-------------|----------------|--|
| SAMPLE UNIT-15 (RANDOM) | | SAMPLE SIZE- 5000 SF | | SAMPLE PCI- 82 | |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 48 LONG/TRANS CR | LOW | 340 LF | 6.80 | 17.6 | |

| | | | | | |
|------------------------|----------|----------------------|-------------|----------------|--|
| SAMPLE UNIT-3 (RANDOM) | | SAMPLE SIZE- 5000 SF | | SAMPLE PCI- 83 | |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 48 LONG/TRANS CR | LOW | 320 LF | 6.40 | 16.8 | |

| | | | | | |
|------------------------|----------|----------------------|-------------|----------------|--|
| SAMPLE UNIT-8 (RANDOM) | | SAMPLE SIZE- 5000 SF | | SAMPLE PCI- 84 | |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 48 LONG/TRANS CR | LOW | 305 LF | 6.10 | 16.2 | |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 5665 LF | 6.43 | 16.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

 BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 1760 LF
 BRANCH NUMBER - R06C SECTION WIDTH - 50 LF
 SECTION NUMBER - 3 SECTION AREA - 9777 SY

INSPECTION DATE - 06/02/89 PCI= 84 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 18
 NUMBER OF SAMPLES SURVEYED= 3
 RECOMMENDED SAMPLES TO BE SURVEYED= 6
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 3.2

SAMPLE UNIT-12 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 88

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 195 LF | 3.90 | 11.7 |

 SAMPLE UNIT-15 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 82

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 360 LF | 7.20 | 18.3 |

 SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 83

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 320 LF | 6.40 | 16.8 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 5136 LF | 5.83 | 15.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

 BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 90 LF
 BRANCH NUMBER - R07C SECTION WIDTH - 50 LF
 SECTION NUMBER - 1 SECTION AREA - 500 SY

INSPECTION DATE - 06/02/89 PCI= 88 RATING= EXCELLENT
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 1
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 88

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 200 LF | 4.00 | 11.9 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 180 LF | 4.00 | 11.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 90 LF
BRANCH NUMBER - R07C SECTION WIDTH - 50 LF
SECTION NUMBER - 2 SECTION AREA - 500 SY

INSPECTION DATE - 06/02/89 PCI= 84 RATING= VERY GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 1
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 84

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 295 LF | 5.90 | 15.8 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 266 LF | 5.91 | 15.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 90 LF
BRANCH NUMBER - R07C SECTION WIDTH - 50 LF
SECTION NUMBER - 3 SECTION AREA - 500 SY

 INSPECTION DATE - 06/02/89 PCI= 79 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 1
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 79

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 448 LF | 8.96 | 21.5 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 403 LF | 8.95 | 21.5 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

 BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 600 LF
 BRANCH NUMBER - R08C SECTION WIDTH - 50 LF
 SECTION NUMBER - 1 SECTION AREA - 3333 SY

INSPECTION DATE - 06/02/89 PCI= 96 RATING= EXCELLENT
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 6
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMENDED SAMPLES TO BE SURVEYED= 5
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 0.7

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 95

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 50 LF | 1.00 | 5.5 |

 SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 96

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 30 LF | 0.60 | 4.1 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 240 LF | 0.80 | 4.7 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |

OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.

BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 600 LF
BRANCH NUMBER - R08C SECTION WIDTH - 50 LF
SECTION NUMBER - 2 SECTION AREA - 3333 SY

INSPECTION DATE - 06/02/89 PCI= 91 RATING= EXCELLENT
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 6
NUMBER OF SAMPLES SURVEYED= 2
RECOMMENDED SAMPLES TO BE SURVEYED= 6
STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 2.1

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 89

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 185 LF | 3.70 | 11.3 |

SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 92

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 105 LF | 2.10 | 8.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 870 LF | 2.90 | 9.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 600 LF
BRANCH NUMBER - R08C SECTION WIDTH - 50 LF
SECTION NUMBER - 3 SECTION AREA - 3333 SY

INSPECTION DATE - 06/02/89 PCI= 92 RATING= EXCELLENT
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 6
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 92

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 105 LF | 2.10 | 8.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 630 LF | 2.10 | 8.2 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|------------------|----------|
| BRANCH NAME - | RUNWAY 13-31 | SECTION LENGTH - | 3600 LF |
| BRANCH NUMBER - | R09C | SECTION WIDTH - | 50 LF |
| SECTION NUMBER - | 1 | SECTION AREA - | 20000 SY |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 91 | RATING= | EXCELLENT |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 36 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 10 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 5 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 1.5 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-10 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 92 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 110 LF | 2.20 | 8.4 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-15 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 92 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 100 LF | 2.00 | 8.0 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-17 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 91 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 125 LF | 2.50 | 9.0 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-20 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 92 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 105 LF | 2.10 | 8.2 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-22 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 91 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 125 LF | 2.50 | 9.0 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-25 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 91 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 115 LF | 2.30 | 8.6 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-31 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 90 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 140 LF | 2.80 | 9.6 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-36 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 87 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 225 LF | 4.50 | 12.9 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-5 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 92 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 100 LF | 2.00 | 8.0 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-7 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 92 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 100 LF | 2.00 | 8.0 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 4482 LF | 2.49 | 8.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|------------------|----------|
| BRANCH NAME - | RUNWAY 13-31 | SECTION LENGTH - | 3600 LF |
| BRANCH NUMBER - | R09C | SECTION WIDTH - | 50 LF |
| SECTION NUMBER - | 2 | SECTION AREA - | 20000 SY |

| | | |
|--|---------|-------------------|
| INSPECTION DATE - 06/02/89 | PCI= 90 | RATING= EXCELLENT |
| CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- | | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 36 |
| NUMBER OF SAMPLES SURVEYED= | | 4 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | 5 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | 2.6 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-14 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 94 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 65 LF | 1.30 | 6.1 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 89 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 175 LF | 3.50 | 10.9 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-24 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 90 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 145 LF | 2.90 | 9.8 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-34 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 88 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 200 LF | 4.00 | 11.9 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 5265 LF | 2.92 | 9.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | |
|----------------------------|--------------------------|
| BRANCH NAME - RUNWAY 13-31 | SECTION LENGTH - 3600 LF |
| BRANCH NUMBER - R09C | SECTION WIDTH - 50 LF |
| SECTION NUMBER - 3 | SECTION AREA - 20000 SY |

| | | |
|--|---------|-------------------|
| INSPECTION DATE - 06/02/89 | PCI= 93 | RATING= EXCELLENT |
| CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- | | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 36 |
| NUMBER OF SAMPLES SURVEYED= | | 4 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | 5 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | 2.9 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-11 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 96 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 25 LF | 0.50 | 3.9 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 94 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 75 LF | 1.50 | 6.5 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-20 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 89 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 185 LF | 3.70 | 11.3 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-28 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 94 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 70 LF | 1.40 | 6.3 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 3195 LF | 1.77 | 7.3 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|------------------|---------|
| BRANCH NAME - | RUNWAY 13-31 | SECTION LENGTH - | 700 LF |
| BRANCH NUMBER - | R10A | SECTION WIDTH - | 50 LF |
| SECTION NUMBER - | 1 | SECTION AREA - | 3888 SY |

INSPECTION DATE - 06/02/89 PCI= 81 RATING= VERY GOOD

| | | | | |
|-------------------------------------|---------|-----------|------------|----------|
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | 7 |
| NUMBER OF SAMPLES SURVEYED= | | | | 2 |

RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 81

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 375 LF | 7.50 | 18.9 |

SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 81

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 385 LF | 7.70 | 19.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 2660 LF | 7.60 | 19.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|------------------|---------|
| BRANCH NAME - | RUNWAY 13-31 | SECTION LENGTH - | 700 LF |
| BRANCH NUMBER - | R10A | SECTION WIDTH - | 50 LF |
| SECTION NUMBER - | 2 | SECTION AREA - | 3888 SY |

INSPECTION DATE - 06/02/89 PCI= 83 RATING= VERY GOOD

| | | | | |
|--|---------|-----------|------------|----------|
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | 7 |
| NUMBER OF SAMPLES SURVEYED= | | | | 2 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | 5 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | 1.4 |

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 84

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 295 LF | 5.90 | 15.8 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-6 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 82 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 340 LF | 6.80 | 17.6 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 2223 LF | 6.35 | 16.7 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|------------------|---------|
| BRANCH NAME - | RUNWAY 13-31 | SECTION LENGTH - | 700 LF |
| BRANCH NUMBER - | R10A | SECTION WIDTH - | 50 LF |
| SECTION NUMBER - | 3 | SECTION AREA - | 3888 SY |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 86 | RATING= | EXCELLENT |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 7 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 2 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 7 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 3.5 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 83 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 320 LF | 6.40 | 16.8 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-4 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 88 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 210 LF | 4.20 | 12.3 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 1855 LF | 5.30 | 14.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

 BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF
 BRANCH NUMBER - R11A SLAB WIDTH - 12.5 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 96

INSPECTION DATE - 06/02/89 PCI= 44 RATING= FAIR
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 1
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 48

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 5.00 | 4.0 |
| 62 CORNER BR | MEDIUM | 1 SLABS | 5.00 | 8.2 |
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 74 JOINT SPALLING | LOW | 6 SLABS | 30.00 | 7.8 |
| 74 JOINT SPALLING | MEDIUM | 5 SLABS | 25.00 | 16.0 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 4 SLABS | 20.00 | 10.0 |
| 73 SHRINKAGE CR | N/A | 8 SLABS | 40.00 | 5.9 |
| 66 SMALL PATCH | LOW | 5 SLABS | 25.00 | 3.2 |

 SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 40

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | HIGH | 1 SLABS | 5.00 | 12.5 |
| 74 JOINT SPALLING | HIGH | 6 SLABS | 30.00 | 34.9 |
| 74 JOINT SPALLING | LOW | 3 SLABS | 15.00 | 4.7 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 73 SHRINKAGE CR | N/A | 6 SLABS | 30.00 | 4.2 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | HIGH | 2 SLABS | 2.08 | 5.2 |
| 62 CORNER BR | LOW | 2 SLABS | 2.08 | 1.5 |
| 62 CORNER BR | MEDIUM | 2 SLABS | 2.08 | 3.6 |
| 75 CORNER SPALLING | HIGH | 2 SLABS | 2.08 | 2.4 |
| 74 JOINT SPALLING | HIGH | 17 SLABS | 17.70 | 27.8 |
| 74 JOINT SPALLING | LOW | 22 SLABS | 22.91 | 6.4 |
| 74 JOINT SPALLING | MEDIUM | 14 SLABS | 14.58 | 10.6 |
| 65 JT SEAL DAMAGE | HIGH | 96 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 10 SLABS | 10.41 | 6.1 |
| 63 LINEAR CR | LOW | 2 SLABS | 2.08 | 2.0 |
| 73 SHRINKAGE CR | N/A | 34 SLABS | 35.41 | 5.0 |
| 66 SMALL PATCH | LOW | 17 SLABS | 17.70 | 1.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 14.56 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 14.20 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = 71.24 PERCENT DEDUCT VALUES.

 BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF
 BRANCH NUMBER - R11A SLAB WIDTH - 12.5 LF
 SECTION NUMBER - 2 NUMBER OF SLABS - 96

INSPECTION DATE - 06/02/89 PCI= 49 RATING= FAIR
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 5
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 16 SLABS SAMPLE PCI- 57

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | MEDIUM | 1 SLABS | 6.25 | 10.3 |
| 74 JOINT SPALLING | LOW | 2 SLABS | 12.50 | 4.1 |
| 74 JOINT SPALLING | MEDIUM | 2 SLABS | 12.50 | 9.5 |
| 65 JT SEAL DAMAGE | HIGH | 16 SLABS | 100.00 | 12.0 |
| 57 LG PATCH/UTIL | MEDIUM | 1 SLABS | 6.25 | 12.8 |
| 73 SHRINKAGE CR | N/A | 12 SLABS | 75.00 | 11.2 |
| 66 SMALL PATCH | LOW | 4 SLABS | 25.00 | 3.2 |

 SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 41

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | HIGH | 1 SLABS | 5.00 | 12.5 |
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 74 JOINT SPALLING | HIGH | 2 SLABS | 10.00 | 20.8 |
| 74 JOINT SPALLING | LOW | 3 SLABS | 15.00 | 4.7 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 73 SHRINKAGE CR | N/A | 6 SLABS | 30.00 | 4.2 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | HIGH | 3 SLABS | 3.12 | 7.8 |
| 62 CORNER BR | MEDIUM | 3 SLABS | 3.12 | 5.1 |
| 75 CORNER SPALLING | HIGH | 3 SLABS | 3.12 | 3.4 |
| 75 CORNER SPALLING | LOW | 3 SLABS | 3.12 | 1.1 |
| 74 JOINT SPALLING | HIGH | 5 SLABS | 5.20 | 13.6 |
| 74 JOINT SPALLING | LOW | 13 SLABS | 13.54 | 4.4 |
| 74 JOINT SPALLING | MEDIUM | 8 SLABS | 8.33 | 6.9 |
| 65 JT SEAL DAMAGE | HIGH | 96 SLABS | 100.00 | 12.0 |

| | | | | |
|------------------|--------|----------|-------|------|
| 67 LG PATCH/UTIL | MEDIUM | 3 SLABS | 3.12 | 7.5 |
| 63 LINEAR CR | HIGH | 3 SLABS | 3.12 | 11.3 |
| 63 LINEAR CR | LOW | 3 SLABS | 3.12 | 3.1 |
| 73 SHRINKAGE CR | N/A | 48 SLABS | 50.00 | 7.4 |
| 66 SMALL PATCH | LOW | 11 SLABS | 11.45 | 1.1 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 32.23 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 14.17 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 53.60 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------|-------------------|---------|
| BRANCH NAME - | RUNWAY 13-31 | SLAB LENGTH - | 12.5 LF |
| BRANCH NUMBER - | R11A | SLAB WIDTH - | 12.5 LF |
| SECTION NUMBER - | 3 | NUMBER OF SLABS - | 96 |

INSPECTION DATE - 06/02/89 PCI= 76 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 5
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 83

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 73 SHRINKAGE CR | N/A | 5 SLABS | 25.00 | 3.5 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 |

SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 69

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | MEDIUM | 1 SLABS | 5.00 | 8.2 |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | MEDIUM | 2 SLABS | 2.08 | 3.6 |
| 75 CORNER SPALLING | HIGH | 2 SLABS | 2.08 | 2.4 |
| 74 JOINT SPALLING | HIGH | 2 SLABS | 2.08 | 6.7 |
| 74 JOINT SPALLING | LOW | 5 SLABS | 5.20 | 2.3 |
| 74 JOINT SPALLING | MEDIUM | 2 SLABS | 2.08 | 2.1 |
| 65 JT SEAL DAMAGE | HIGH | 96 SLABS | 100.00 | 12.0 |
| 73 SHRINKAGE CR | N/A | 12 SLABS | 12.50 | 1.8 |
| 66 SMALL PATCH | LOW | 2 SLABS | 2.08 | 0.3 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 11.54 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 38.46 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 50.00 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|----------------------|------------------|----------|
| BRANCH NAME - | RUNWAY 04-22 OVERRUN | SECTION LENGTH - | 850 LF |
| BRANCH NUMBER - | 001C | SECTION WIDTH - | 150 LF |
| SECTION NUMBER - | 1 | SECTION AREA - | 14166 SY |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 78 | RATING= | VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 24 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 4 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 14 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 8.7 |

| | | | | |
|-------------------------|--------------|---------|-------------|----|
| SAMPLE UNIT-17 (RANDOM) | SAMPLE SIZE- | 5000 SF | SAMPLE PCI- | 71 |
|-------------------------|--------------|---------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 1200 SF | 24.00 | 22.5 |
| 48 LONG/TRANS CR | LOW | 460 LF | 9.20 | 22.0 |

| | | | | |
|------------------------|--------------|---------|-------------|----|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- | 7000 SF | SAMPLE PCI- | 84 |
|------------------------|--------------|---------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 600 SF | 8.57 | 16.3 |
| 48 LONG/TRANS CR | LOW | 210 LF | 3.00 | 10.0 |

| | | | | |
|-------------------------|--------------|---------|-------------|----|
| SAMPLE UNIT-24 (RANDOM) | SAMPLE SIZE- | 5000 SF | SAMPLE PCI- | 69 |
|-------------------------|--------------|---------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 2500 SF | 50.00 | 28.5 |
| 48 LONG/TRANS CR | LOW | 385 LF | 7.70 | 19.2 |

| | | | | |
|------------------------|--------------|---------|-------------|----|
| SAMPLE UNIT-7 (RANDOM) | SAMPLE SIZE- | 5000 SF | SAMPLE PCI- | 86 |
|------------------------|--------------|---------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 260 LF | 5.20 | 14.4 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 24940 SF | 19.56 | 20.8 |
| 48 LONG/TRANS CR | LOW | 7627 LF | 5.98 | 15.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | | | |
|---------------|--------------------------|------------------|--------|
| BRANCH NAME - | RUNWAY 04-22 O'RUN BLAST | SECTION LENGTH - | 150 LF |
|---------------|--------------------------|------------------|--------|

BRANCH NUMBER - 002C SECTION WIDTH - 150 LF
SECTION NUMBER - 1 SECTION AREA - 2500 SY

INSPECTION DATE - 06/02/89 PCI= 84 RATING= VERY GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 4
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 84

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 270 LF | 4.80 | 13.5 |
| 48 LONG/TRANS CR | MEDIUM | 90 LF | 1.60 | 14.6 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 1080 LF | 4.80 | 13.5 |
| 48 LONG/TRANS CR | MEDIUM | 360 LF | 1.60 | 14.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.

BRANCH NAME - RUNWAY 04-22 O'RUN BLAST SECTION LENGTH - 150 LF
BRANCH NUMBER - 003C SECTION WIDTH - 150 LF
SECTION NUMBER - 1 SECTION AREA - 2500 SY

INSPECTION DATE - 06/02/89 PCI= 80 RATING= VERY GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 1
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 80

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | HIGH | 60 LF | 1.06 | 20.5 |
| 48 LONG/TRANS CR | LOW | 165 LF | 2.93 | 9.8 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | HIGH | 240 LF | 1.06 | 20.5 |
| 48 LONG/TRANS CR | LOW | 660 LF | 2.93 | 9.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCENT DEDUCT VALUES.

OTHER

RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.

 BRANCH NAME - RUNWAY 04-22 OVERRUN SECTION LENGTH - 850 LF
 BRANCH NUMBER - 004C SECTION WIDTH - 150 LF
 SECTION NUMBER - 1 SECTION AREA - 14166 SY

INSPECTION DATE - 06/02/89 PCI= 86 RATING= EXCELLENT
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 24
 NUMBER OF SAMPLES SURVEYED= 4
 RECOMMENDED SAMPLES TO BE SURVEYED= 15
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 9.3

SAMPLE UNIT-11 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 78

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 1200 SF | 24.00 | 22.5 |
| 48 LONG/TRANS CR | LOW | 125 LF | 2.50 | 9.0 |

SAMPLE UNIT-18 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 96

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 42 BLEEDING | N/A | 10 SF | 0.20 | 1.0 |
| 48 LONG/TRANS CR | LOW | 20 LF | 0.40 | 3.5 |

SAMPLE UNIT-23 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 78

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 42 BLEEDING | N/A | 30 SF | 0.60 | 3.6 |
| 43 BLOCK CR | LOW | 1200 SF | 24.00 | 22.5 |
| 48 LONG/TRANS CR | LOW | 75 LF | 1.50 | 6.5 |

SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 92

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 110 LF | 2.20 | 8.4 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 42 BLEEDING | N/A | 255 SF | 0.20 | 1.0 |
| 43 BLOCK CR | LOW | 15288 SF | 11.99 | 18.1 |
| 48 LONG/TRANS CR | LOW | 2102 LF | 1.64 | 6.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 96.15 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = 3.85 PERCENT DEDUCT VALUES.

 BRANCH NAME - RUNWAY 13-31 O'RUN BLAST SECTION LENGTH - 150 LF

BRANCH NUMBER - 005C SECTION WIDTH - 150 LF
SECTION NUMBER - 1 SECTION AREA - 2500 SY

INSPECTION DATE - 06/02/89 PCI= 64 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 4
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 64

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 5625 SF | 100.00 | 35.7 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 22500 SF | 100.00 | 35.7 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.

BRANCH NAME - RUNWAY 13-31 OVERRUN SECTION LENGTH - 850 LF
BRANCH NUMBER - 006C SECTION WIDTH - 150 LF
SECTION NUMBER - 1 SECTION AREA - 14166 SY

INSPECTION DATE - 06/02/89 PCI= 66 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 24
NUMBER OF SAMPLES SURVEYED= 4
RECOMMENDED SAMPLES TO BE SURVEYED= 5
STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 3.5

SAMPLE UNIT-12 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 71

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 2800 SF | 56.00 | 29.4 |

SAMPLE UNIT-14 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 64

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 5000 SF | 100.00 | 35.7 |

SAMPLE UNIT-19 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 64

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 5000 SF | 100.00 | 35.7 |

SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 64

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 5000 SF | 100.00 | 35.7 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|-----------|-------------|--------------|
| 43 BLOCK CR | LOW | 113386 SF | 88.93 | 34.2 |

*** PERCENT DEDUCT BASED ON VALUES ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

 BRANCH NAME - RUNWAY 13-31 O'RUN BLAST SECTION LENGTH - 150 LF
 BRANCH NUMBER - 007C SECTION WIDTH - 150 LF
 SECTION NUMBER - 1 SECTION AREA - 2500 SY

INSPECTION DATE - 06/02/89 PCI= 58 RATING= GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 4
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 58

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 4100 SF | 72.88 | 31.9 |
| 43 BLOCK CR | MEDIUM | 1200 SF | 21.33 | 30.4 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 16400 SF | 72.88 | 31.9 |
| 43 BLOCK CR | MEDIUM | 4800 SF | 21.33 | 30.4 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

 BRANCH NAME - RUNWAY 13-31 OVERRUN SECTION LENGTH - 850 LF
 BRANCH NUMBER - 008C SECTION WIDTH - 150 LF
 SECTION NUMBER - 1 SECTION AREA - 14166 SY

INSPECTION DATE - 06/02/89 PCI= 92 RATING= EXCELLENT
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 24
 NUMBER OF SAMPLES SURVEYED= 4
 RECOMMENDED SAMPLES TO BE SURVEYED= 17

STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 11.8

SAMPLE UNIT-14 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 94

DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE
48 LONG/TRANS CR LOW 75 LF 1.50 6.5

SAMPLE UNIT-19 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 75

DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE
43 BLOCK CR LOW 1800 SF 36.00 25.5
48 LONG/TRANS CR LOW 275 LF 5.50 15.0

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 7500 SF SAMPLE PCI-100

NO DISTRESS

SAMPLE UNIT-9 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI-100

NO DISTRESS

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 10206 SF | 8.00 | 16.0 |
| 48 LONG/TRANS CR | LOW | 1985 LF | 1.55 | 6.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|-------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 100.00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

BRANCH NAME - TAXIWAYS R AND A SLAB LENGTH - 25.0 LF
BRANCH NUMBER - T03A SLAB WIDTH - 25.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 336

INSPECTION DATE - 06/02/89 PCI= 59 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 17
NUMBER OF SAMPLES SURVEYED= 3
RECOMMENDED SAMPLES TO BE SURVEYED= 16
STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 14.9

SAMPLE UNIT-15 (RANDOM) SAMPLE SIZE- 21 SLABS SAMPLE PCI- 55

DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE
74 JOINT SPALLING LOW 1 SLABS 4.76 2.1
74 JOINT SPALLING MEDIUM 1 SLABS 4.76 4.3
65 JT SEAL DAMAGE MEDIUM 21 SLABS 100.00 7.0
67 LG PATCH/UTIL MEDIUM 1 SLABS 4.76 10.6
63 LINEAR CR MEDIUM 6 SLABS 28.57 34.0

| | | | | |
|----------------|-----|---------|-------|-----|
| 66 SMALL PATCH | LOW | 3 SLABS | 14.28 | 1.5 |
|----------------|-----|---------|-------|-----|

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-3 (RANDOM) | SAMPLE SIZE- | 21 SLABS | SAMPLE PCI- | 76 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 21 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | HIGH | 1 SLABS | 4.76 | 15.7 |
| 63 LINEAR CR | LOW | 1 SLABS | 4.76 | 4.6 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 9.52 | 1.4 |
| 66 SMALL PATCH | LOW | 1 SLABS | 4.76 | 0.6 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-9 (RANDOM) | SAMPLE SIZE- | 21 SLABS | SAMPLE PCI- | 47 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 2 SLABS | 9.52 | 3.5 |
| 74 JOINT SPALLING | LOW | 2 SLABS | 9.52 | 3.3 |
| 65 JT SEAL DAMAGE | HIGH | 21 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 2 SLABS | 9.52 | 5.7 |
| 63 LINEAR CR | HIGH | 1 SLABS | 4.76 | 15.7 |
| 63 LINEAR CR | MEDIUM | 4 SLABS | 19.04 | 27.4 |
| 66 SMALL PATCH | LOW | 2 SLABS | 9.52 | 1.0 |
| 66 SMALL PATCH | MEDIUM | 4 SLABS | 19.04 | 9.1 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 11 SLABS | 3.27 | 1.2 |
| 74 JOINT SPALLING | LOW | 16 SLABS | 4.76 | 2.1 |
| 74 JOINT SPALLING | MEDIUM | 5 SLABS | 1.48 | 1.5 |
| 65 JT SEAL DAMAGE | HIGH | 112 SLABS | 33.33 | 12.0 |
| 65 JT SEAL DAMAGE | MEDIUM | 224 SLABS | 66.66 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 11 SLABS | 3.27 | 2.1 |
| 67 LG PATCH/UTIL | MEDIUM | 5 SLABS | 1.48 | 3.7 |
| 63 LINEAR CR | HIGH | 11 SLABS | 3.27 | 11.8 |
| 63 LINEAR CR | LOW | 5 SLABS | 1.48 | 1.4 |
| 63 LINEAR CR | MEDIUM | 53 SLABS | 15.77 | 24.7 |
| 73 SHRINKAGE CR | N/A | 11 SLABS | 3.27 | 0.9 |
| 66 SMALL PATCH | LOW | 32 SLABS | 9.52 | 1.0 |
| 66 SMALL PATCH | MEDIUM | 21 SLABS | 6.25 | 3.3 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 52.13 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 26.13 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 21.73 PERCENT DEDUCT VALUES. |

| | |
|-------------------------|-----------------------|
| BRANCH NAME - TAXIWAY A | SLAB LENGTH - 25.0 LF |
| BRANCH NUMBER - T04A | SLAB WIDTH - 25.0 LF |
| SECTION NUMBER - 1 | NUMBER OF SLABS - 149 |

| | | |
|----------------------------|---------|-------------------------------|
| INSPECTION DATE - 06/02/89 | PCI= 80 | RATING= VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- SHOULDERS- OVERALL- |

| | |
|--|-----|
| TOTAL NUMBER OF SAMPLES IN SECTION= | 7 |
| NUMBER OF SAMPLES SURVEYED= | 2 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | 5 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | 0.7 |

| | | | | |
|------------------------|----------|--------------|-------------|----------------|
| SAMPLE UNIT-3 (RANDOM) | | SAMPLE SIZE- | 21 SLABS | SAMPLE PCI- 79 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 74 JOINT SPALLING | LOW | 3 SLABS | 14.28 | 4.5 |
| 65 JT SEAL DAMAGE | HIGH | 21 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 2 SLABS | 9.52 | 8.1 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 4.76 | 1.0 |
| 66 SMALL PATCH | LOW | 1 SLABS | 4.76 | 0.6 |

| | | | | |
|------------------------|----------|--------------|-------------|----------------|
| SAMPLE UNIT-6 (RANDOM) | | SAMPLE SIZE- | 21 SLABS | SAMPLE PCI- 80 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | LOW | 1 SLABS | 4.76 | 1.8 |
| 65 JT SEAL DAMAGE | HIGH | 21 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 1 SLABS | 4.76 | 4.6 |
| 66 SMALL PATCH | LOW | 9 SLABS | 42.85 | 6.0 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| | | | | |
|--------------------|----------|-----------|-------------|--------------|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | LOW | 4 SLABS | 2.68 | 0.9 |
| 74 JOINT SPALLING | LOW | 11 SLABS | 7.38 | 2.8 |
| 65 JT SEAL DAMAGE | HIGH | 149 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 11 SLABS | 7.38 | 6.6 |
| 73 SHRINKAGE CR | N/A | 4 SLABS | 2.68 | 0.8 |
| 66 SMALL PATCH | LOW | 36 SLABS | 24.16 | 3.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 25.29 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 45.98 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 28.74 PERCENT DEDUCT VALUES. |

| | |
|-------------------------|-----------------------|
| BRANCH NAME - TAXIWAY B | SLAB LENGTH - 12.5 LF |
| BRANCH NUMBER - T05B | SLAB WIDTH - 20.0 LF |
| SECTION NUMBER - 1 | NUMBER OF SLABS - 114 |

| | | |
|--|---------|--------------|
| INSPECTION DATE - 06/02/89 | PCI= 65 | RATING= GOOD |
| CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- | | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | 6 | |
| NUMBER OF SAMPLES SURVEYED= | 2 | |
| RECOMMENDED SAMPLES TO BE SURVEYED= | 6 | |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | 6.3 | |

| | | | | |
|------------------------|----------|--------------|-------------|----------------|
| SAMPLE UNIT-1 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 69 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |

| | | | | |
|--------------------|--------|---------|-------|------|
| 75 CORNER SPALLING | HIGH | 3 SLABS | 15.00 | 12.6 |
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 |
| 73 SHRINKAGE CR | N/A | 6 SLABS | 30.00 | 4.2 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 60 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 10.00 | 19.0 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 10.00 | 1.5 |
| 66 SMALL PATCH | LOW | 6 SLABS | 30.00 | 4.0 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 9 SLABS | 7.89 | 7.3 |
| 75 CORNER SPALLING | LOW | 3 SLABS | 2.63 | 0.9 |
| 74 JOINT SPALLING | HIGH | 3 SLABS | 2.63 | 8.0 |
| 74 JOINT SPALLING | MEDIUM | 3 SLABS | 2.63 | 2.6 |
| 67 LG PATCH/UTIL | LOW | 3 SLABS | 2.63 | 1.8 |
| 63 LINEAR CR | HIGH | 3 SLABS | 2.63 | 9.8 |
| 73 SHRINKAGE CR | N/A | 23 SLABS | 20.17 | 2.9 |
| 66 SMALL PATCH | LOW | 17 SLABS | 14.91 | 1.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 50.98 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 49.02 PERCENT DEDUCT VALUES. |

| | | |
|-------------------------|------------------|---------|
| BRANCH NAME - TAXIWAY B | SECTION LENGTH - | 860 LF |
| BRANCH NUMBER - T06B | SECTION WIDTH - | 75 LF |
| SECTION NUMBER - 1 | SECTION AREA - | 7166 SY |

| | | |
|--|---------|--------------|
| INSPECTION DATE - 06/02/89 | PCI= 59 | RATING= GOOD |
| CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- | | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 11 |
| NUMBER OF SAMPLES SURVEYED= | | 3 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | 7 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | 4.7 |

| | | | | |
|-------------------------|--------------|---------|-------------|----|
| SAMPLE UNIT-10 (RANDOM) | SAMPLE SIZE- | 5625 SF | SAMPLE PCI- | 55 |
|-------------------------|--------------|---------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 480 SF | 8.53 | 41.3 |
| 43 BLOCK CR | LOW | 600 SF | 10.66 | 17.5 |
| 48 LONG/TRANS CR | LOW | 260 LF | 4.62 | 13.2 |
| 48 LONG/TRANS CR | MEDIUM | 80 LF | 1.42 | 13.7 |

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 64

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 5625 SF | 100.00 | 35.7 |

SAMPLE UNIT-6 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 57

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 340 SF | 6.04 | 37.9 |
| 48 LONG/TRANS CR | LOW | 475 LF | 8.44 | 20.6 |
| 48 LONG/TRANS CR | MEDIUM | 55 LF | 0.97 | 11.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 3132 SF | 4.85 | 35.8 |
| 43 BLOCK CR | LOW | 23780 SF | 36.87 | 25.7 |
| 48 LONG/TRANS CR | LOW | 2808 LF | 4.35 | 12.6 |
| 48 LONG/TRANS CR | MEDIUM | 516 LF | 0.80 | 10.1 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 42.52 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 57.48 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

BRANCH NAME - NORTH CAL PAD ACCESS SECTION LENGTH - 1925 LF
BRANCH NUMBER - T07C SECTION WIDTH - 75 LF
SECTION NUMBER - 1 SECTION AREA - 16041 SY

INSPECTION DATE - 06/02/89 PCI= 45 RATING= FAIR
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 19
NUMBER OF SAMPLES SURVEYED= 3
RECOMMENDED SAMPLES TO BE SURVEYED= 16
STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 11.5

SAMPLE UNIT-11 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 33

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 600 SF | 12.00 | 18.1 |
| 43 BLOCK CR | MEDIUM | 200 SF | 4.00 | 17.9 |
| 48 LONG/TRANS CR | HIGH | 60 LF | 1.20 | 21.6 |
| 48 LONG/TRANS CR | LOW | 375 LF | 7.50 | 18.9 |
| 48 LONG/TRANS CR | MEDIUM | 170 LF | 3.40 | 20.8 |
| 53 RUTTING | MEDIUM | 400 SF | 8.00 | 41.1 |

SAMPLE UNIT-18 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 56

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 850 SF | 17.00 | 20.1 |

| | | | | |
|------------------|--------|--------|------|------|
| 48 LONG/TRANS CR | HIGH | 85 LF | 1.70 | 25.6 |
| 48 LONG/TRANS CR | LOW | 330 LF | 6.60 | 17.2 |
| 48 LONG/TRANS CR | MEDIUM | 190 LF | 3.80 | 21.8 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-4 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 45 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | MEDIUM | 400 SF | 8.00 | 22.1 |
| 48 LONG/TRANS CR | HIGH | 40 LF | 0.80 | 18.0 |
| 48 LONG/TRANS CR | LOW | 235 LF | 4.70 | 13.3 |
| 48 LONG/TRANS CR | MEDIUM | 100 LF | 2.00 | 16.3 |
| 53 RUTTING | MEDIUM | 200 SF | 4.00 | 34.8 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 13949 SF | 9.66 | 17.0 |
| 43 BLOCK CR | MEDIUM | 5772 SF | 3.99 | 17.8 |
| 48 LONG/TRANS CR | HIGH | 1780 LF | 1.23 | 21.9 |
| 48 LONG/TRANS CR | LOW | 9043 LF | 6.26 | 16.5 |
| 48 LONG/TRANS CR | MEDIUM | 4425 LF | 3.06 | 19.9 |
| 53 RUTTING | MEDIUM | 5772 SF | 3.99 | 34.7 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 27.15 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 72.85 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

| | |
|-------------------------|-----------------------|
| BRANCH NAME - TAXIWAY E | SLAB LENGTH - 20.0 LF |
| BRANCH NUMBER - T08A | SLAB WIDTH - 20.0 LF |
| SECTION NUMBER - 1 | NUMBER OF SLABS - 381 |

| | | |
|--|---------|-------------------|
| INSPECTION DATE - 06/02/89 | PCI= 96 | RATING= EXCELLENT |
| CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- | | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 16 |
| NUMBER OF SAMPLES SURVEYED= | | 5 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | 5 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | 1.7 |

| | | |
|------------------------|-----------------------|----------------|
| SAMPLE UNIT-1 (RANDOM) | SAMPLE SIZE- 20 SLABS | SAMPLE PCI- 98 |
|------------------------|-----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |

| | | |
|------------------------|-----------------------|----------------|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- 20 SLABS | SAMPLE PCI- 98 |
|------------------------|-----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |

| | | |
|------------------------|-----------------------|----------------|
| SAMPLE UNIT-3 (RANDOM) | SAMPLE SIZE- 20 SLABS | SAMPLE PCI- 96 |
|------------------------|-----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-4 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 95 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|----------------|----------|----------|-------------|--------------|
| 66 SMALL PATCH | LOW | 7 SLABS | 35.00 | 5.0 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-5 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 94 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-----------------|----------|----------|-------------|--------------|
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 10.00 | 1.5 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 4 SLABS | 1.04 | 0.3 |
| 74 JOINT SPALLING | LOW | 8 SLABS | 2.09 | 1.3 |
| 65 JT SEAL DAMAGE | LOW | 76 SLABS | 19.94 | 2.0 |
| 63 LINEAR CR | LOW | 4 SLABS | 1.04 | 1.0 |
| 73 SHRINKAGE CR | N/A | 8 SLABS | 2.09 | 0.8 |
| 66 SMALL PATCH | LOW | 27 SLABS | 7.08 | 0.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 15.87 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 31.75 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 52.38 PERCENT DEDUCT VALUES. |

| | | |
|-------------------------------------|-------------------|---------|
| BRANCH NAME - TAXIWAY R - SOUTH END | SLAB LENGTH - | 20.0 LF |
| BRANCH NUMBER - T09A | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - 1 | NUMBER OF SLABS - | 131 |

| | | | | |
|--|---------|-----------|------------|-----------|
| INSPECTION DATE - 06/02/89 | PCI= | 82 | RATING= | VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | 6 |
| NUMBER OF SAMPLES SURVEYED= | | | | 2 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | 6 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | 14.1 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-3 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 72 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | MEDIUM | 3 SLABS | 15.00 | 20.3 |
| 63 LINEAR CR | LOW | 2 SLABS | 10.00 | 8.5 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 10.00 | 1.5 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-5 (RANDOM) | SAMPLE SIZE- | 24 SLABS | SAMPLE PCI- | 92 |
|------------------------|--------------|----------|-------------|----|

OTHER

RELATED DISTRESSES = 23.85 PERCENT DEDUCT VALUES.

 BRANCH NAME - TAXIWAY D SLAB LENGTH - 25.0 LF
 BRANCH NUMBER - T11A SLAB WIDTH - 25.0 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 320

INSPECTION DATE - 06/02/89 PCI= 78 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 15
 NUMBER OF SAMPLES SURVEYED= 3
 RECOMMENDED SAMPLES TO BE SURVEYED= 8
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 4.5

SAMPLE UNIT-13 (RANDOM) SAMPLE SIZE- 21 SLABS SAMPLE PCI- 82

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 21 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 2 SLABS | 9.52 | 8.1 |
| 73 SHRINKAGE CR | N/A | 7 SLABS | 33.33 | 4.7 |
| 66 SMALL PATCH | LOW | 4 SLABS | 19.04 | 2.1 |

SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 21 SLABS SAMPLE PCI- 73

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | HIGH | 21 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 8 SLABS | 38.09 | 18.6 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 9.52 | 1.4 |
| 66 SMALL PATCH | LOW | 3 SLABS | 14.28 | 1.5 |

SAMPLE UNIT-9 (RANDOM) SAMPLE SIZE- 21 SLABS SAMPLE PCI- 79

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 4.76 | 2.1 |
| 65 JT SEAL DAMAGE | MEDIUM | 21 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 5 SLABS | 23.80 | 15.0 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 9.52 | 1.4 |
| 66 SMALL PATCH | LOW | 2 SLABS | 9.52 | 1.0 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|-----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 5 SLABS | 1.56 | 1.0 |
| 65 JT SEAL DAMAGE | HIGH | 107 SLABS | 33.43 | 12.0 |
| 65 JT SEAL DAMAGE | MEDIUM | 213 SLABS | 66.56 | 7.0 |
| 63 LINEAR CR | LOW | 76 SLABS | 23.75 | 15.0 |
| 73 SHRINKAGE CR | N/A | 56 SLABS | 17.50 | 2.4 |
| 66 SMALL PATCH | LOW | 46 SLABS | 14.37 | 1.5 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 38.56 PERCENT DEDUCT VALUES.

CLIMATE/DURABILITY RELATED DISTRESSES = 48.84 PERCENT DEDUCT VALUES.

OTHER

RELATED DISTRESSES = 12.60 PERCENT DEDUCT VALUES.

BRANCH NAME - TAXIWAY C SECTION LENGTH - LF
BRANCH NUMBER - T12B SECTION WIDTH - LF
SECTION NUMBER - 1 SECTION AREA - 9439 SY

INSPECTION DATE - 06/02/89 PCI= 59 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 14
NUMBER OF SAMPLES SURVEYED= 2
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-6 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 59

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 275 LF | 5.50 | 15.0 |

SAMPLE UNIT-9 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 59

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 255 LF | 5.10 | 14.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 11050 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 4505 LF | 5.30 | 14.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.

BRANCH NAME - TAXIWAY C SLAB LENGTH - 25.0 LF
BRANCH NUMBER - T13B SLAB WIDTH - 25.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 34

INSPECTION DATE - 06/02/89 PCI= 85 RATING= VERY GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 2
NUMBER OF SAMPLES SURVEYED= 2
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 21 SLABS SAMPLE PCI- 80

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 4.76 | 2.1 |
| 65 JT SEAL DAMAGE | MEDIUM | 21 SLABS | 100.00 | 7.0 |

| | | | | |
|--------------------|-----|---------|-------|------|
| 63 LINEAR CR | LOW | 4 SLABS | 19.04 | 13.2 |
| 70 SCALING/CRAZING | LOW | 1 SLABS | 4.76 | 2.0 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- | 17 SLABS | SAMPLE PCI- | 89 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.88 | 2.4 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.88 | 5.1 |
| 65 JT SEAL DAMAGE | LOW | 17 SLABS | 100.00 | 2.0 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 11.76 | 1.7 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 2 SLABS | 5.88 | 2.4 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 2.94 | 2.8 |
| 65 JT SEAL DAMAGE | LOW | 15 SLABS | 44.11 | 2.0 |
| 65 JT SEAL DAMAGE | MEDIUM | 19 SLABS | 55.88 | 7.0 |
| 63 LINEAR CR | LOW | 4 SLABS | 11.76 | 9.5 |
| 70 SCALING/CRAZING | LOW | 1 SLABS | 2.94 | 1.3 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 5.88 | 1.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 36.54 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 34.62 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 28.85 PERCENT DEDUCT VALUES. |

| | | |
|-------------------------|------------------|----------|
| BRANCH NAME - TAXIWAY C | SECTION LENGTH - | LF |
| BRANCH NUMBER - T14B | SECTION WIDTH - | LF |
| SECTION NUMBER - 1 | SECTION AREA - | 28782 SY |

| | | |
|--|---------|--------------|
| INSPECTION DATE - 06/02/89 | PCI= 59 | RATING= GOOD |
| CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- | | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 70 |
| NUMBER OF SAMPLES SURVEYED= | | 7 |
| RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. | | |

| | | | | |
|-------------------------|--------------|---------|-------------|----|
| SAMPLE UNIT-16 (RANDOM) | SAMPLE SIZE- | 5000 SF | SAMPLE PCI- | 59 |
|-------------------------|--------------|---------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 150 LF | 3.00 | 10.0 |

| | | | | |
|-------------------------|--------------|---------|-------------|----|
| SAMPLE UNIT-26 (RANDOM) | SAMPLE SIZE- | 5000 SF | SAMPLE PCI- | 59 |
|-------------------------|--------------|---------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 210 LF | 4.20 | 12.3 |
| 48 LONG/TRANS CR | MEDIUM | 40 LF | 0.80 | 10.1 |

| | | | | |
|-------------------------|--------------|---------|-------------|----|
| SAMPLE UNIT-36 (RANDOM) | SAMPLE SIZE- | 5000 SF | SAMPLE PCI- | 59 |
|-------------------------|--------------|---------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 42 BLEEDING | N/A | 20 SF | 0.40 | 2.5 |
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 110 LF | 2.20 | 8.4 |
| 49 OIL SPILLAGE | N/A | 5 SF | 0.10 | 2.0 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-46 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 59 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 71 LF | 1.42 | 6.3 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-56 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 59 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 188 LF | 3.76 | 11.4 |

| | | |
|------------------------|----------------------|----------------|
| SAMPLE UNIT-6 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 59 |
|------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 42 BLEEDING | N/A | 35 SF | 0.70 | 4.2 |
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 80 LF | 1.60 | 6.8 |

| | | |
|-------------------------|----------------------|----------------|
| SAMPLE UNIT-66 (RANDOM) | SAMPLE SIZE- 5000 SF | SAMPLE PCI- 59 |
|-------------------------|----------------------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 42 BLEEDING | N/A | 120 SF | 2.40 | 12.6 |
| 47 JT REFLECT CR | MEDIUM | 650 LF | 13.00 | 40.8 |
| 48 LONG/TRANS CR | LOW | 110 LF | 2.20 | 8.4 |
| 48 LONG/TRANS CR | MEDIUM | 8 LF | 0.16 | 4.6 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 42 BLEEDING | N/A | 1295 SF | 0.49 | 2.9 |
| 47 JT REFLECT CR | MEDIUM | 33670 LF | 12.99 | 40.7 |
| 48 LONG/TRANS CR | LOW | 6801 LF | 2.62 | 9.2 |
| 49 OIL SPILLAGE | N/A | 37 SF | 0.01 | 0.2 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 94.59 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 5.41 PERCENT DEDUCT VALUES. |

| | | |
|----------------------------------|------------------|---------|
| BRANCH NAME - TAXIWAY C - FILLET | SECTION LENGTH - | LF |
| BRANCH NUMBER - T15C | SECTION WIDTH - | LF |
| SECTION NUMBER - 1 | SECTION AREA - | 1313 SY |

| | | |
|----------------------------|---------|--------------|
| INSPECTION DATE - 06/02/89 | PCI= 58 | RATING= GOOD |
|----------------------------|---------|--------------|

CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 3
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

 SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 58

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 707 LF | 14.14 | 42.0 |
| 48 LONG/TRANS CR | MEDIUM | 24 LF | 0.48 | 7.8 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 47 JT REFLECT CR | MEDIUM | 1669 LF | 14.12 | 42.0 |
| 48 LONG/TRANS CR | MEDIUM | 57 LF | 0.48 | 7.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***
 LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.

 BRANCH NAME - TAXIWAY C - PAD ACCESS SECTION LENGTH - 280 LF
 BRANCH NUMBER - T16C SECTION WIDTH - 75 LF
 SECTION NUMBER - 1 SECTION AREA - 2333 SY

INSPECTION DATE - 06/02/89 PCI= 77 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 3
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

 SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 77

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 400 LF | 7.11 | 18.2 |
| 48 LONG/TRANS CR | MEDIUM | 165 LF | 2.93 | 19.5 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 48 LONG/TRANS CR | LOW | 1492 LF | 7.10 | 18.1 |
| 48 LONG/TRANS CR | MEDIUM | 615 LF | 2.92 | 19.5 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***
 LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.

 BRANCH NAME - TAXIWAY C SECTION LENGTH - 2140 LF

BRANCH NUMBER - T17B
SECTION NUMBER - 1

SECTION WIDTH - 75 LF
SECTION AREA - 17833 SY

INSPECTION DATE - 06/02/89 PCI= 69 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 28
NUMBER OF SAMPLES SURVEYED= 4
RECOMMENDED SAMPLES TO BE SURVEYED= 5
STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 3.3

SAMPLE UNIT-12 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 69

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 150 SF | 2.66 | 30.1 |
| 48 LONG/TRANS CR | LOW | 115 LF | 2.04 | 8.0 |
| 48 LONG/TRANS CR | MEDIUM | 110 LF | 1.95 | 16.0 |

SAMPLE UNIT-19 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 64

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 260 SF | 4.62 | 35.3 |
| 48 LONG/TRANS CR | LOW | 10 LF | 0.17 | 2.8 |
| 48 LONG/TRANS CR | MEDIUM | 115 LF | 2.04 | 16.4 |

SAMPLE UNIT-24 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 71

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 139 SF | 2.47 | 29.3 |
| 48 LONG/TRANS CR | LOW | 305 LF | 5.42 | 14.8 |

SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 71

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 137 SF | 2.43 | 29.1 |
| 48 LONG/TRANS CR | LOW | 230 LF | 4.08 | 12.0 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 4891 SF | 3.04 | 31.6 |
| 48 LONG/TRANS CR | LOW | 1706 LF | 2.93 | 9.8 |
| 48 LONG/TRANS CR | MEDIUM | 1604 LF | 0.99 | 11.4 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 59.85 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 40.15 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.

BRANCH NAME - TAXIWAY C
BRANCH NUMBER - T18B
SECTION NUMBER - 1

SECTION LENGTH - LF
SECTION WIDTH - 943 LF
SECTION AREA - 75 SY

 INSPECTION DATE - 06/02/89 PCI= 77 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 12
 NUMBER OF SAMPLES SURVEYED= 3
 RECOMMENDED SAMPLES TO BE SURVEYED= 5
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 3.2

SAMPLE UNIT-10 (RANDOM) SAMPLE SIZE- 4125 SF SAMPLE PCI- 75

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 64 SF | 1.55 | 24.7 |
| 48 LONG/TRANS CR | LOW | 170 LF | 4.12 | 12.1 |

 SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 5625 SF SAMPLE PCI- 81

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 150 SF | 2.66 | 10.7 |
| 43 BLOCK CR | MEDIUM | 75 SF | 1.33 | 12.9 |
| 48 LONG/TRANS CR | LOW | 190 LF | 3.37 | 10.7 |
| 48 LONG/TRANS CR | MEDIUM | 55 LF | 0.97 | 11.2 |

 SAMPLE UNIT-7 (RANDOM) SAMPLE SIZE- 4125 SF SAMPLE PCI- 76

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 43 BLOCK CR | LOW | 48 SF | 1.16 | 8.2 |
| 43 BLOCK CR | MEDIUM | 150 SF | 3.63 | 17.1 |
| 48 LONG/TRANS CR | LOW | 318 LF | 7.70 | 19.2 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 41 ALLIGATOR CR | LOW | 3 SF | 0.44 | 13.2 |
| 43 BLOCK CR | LOW | 10 SF | 1.48 | 8.9 |
| 43 BLOCK CR | MEDIUM | 11 SF | 1.62 | 13.6 |
| 48 LONG/TRANS CR | LOW | 34 LF | 5.03 | 14.0 |
| 48 LONG/TRANS CR | MEDIUM | 3 LF | 0.44 | 7.4 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 23.12 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 76.88 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |

 BRANCH NAME - TAXIWAY R AT SOUTH CAL SLAB LENGTH - 25.0 LF
 BRANCH NUMBER - T19A SLAB WIDTH - 20.0 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 57

INSPECTION DATE - 06/02/89 PCI= 62 RATING= GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 3
 NUMBER OF SAMPLES SURVEYED= 1

RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 18 SLABS SAMPLE PCI- 62

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 18 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 2 SLABS | 11.11 | 6.4 |
| 67 LG PATCH/UTIL | MEDIUM | 2 SLABS | 11.11 | 17.6 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 11.11 | 20.1 |
| 66 SMALL PATCH | LOW | 5 SLABS | 27.77 | 3.6 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 57 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 6 SLABS | 10.52 | 6.2 |
| 67 LG PATCH/UTIL | MEDIUM | 6 SLABS | 10.52 | 17.1 |
| 63 LINEAR CR | MEDIUM | 6 SLABS | 10.52 | 19.5 |
| 66 SMALL PATCH | LOW | 16 SLABS | 28.07 | 3.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 36.52 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 13.11 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 50.37 PERCENT DEDUCT VALUES. |

| | |
|-------------------------|-----------------------|
| BRANCH NAME - TAXIWAY R | SLAB LENGTH - 25.0 LF |
| BRANCH NUMBER - T20A | SLAB WIDTH - 25.0 LF |
| SECTION NUMBER - 1 | NUMBER OF SLABS - 432 |

| | | |
|--|---------|--------------|
| INSPECTION DATE - 06/02/89 | PCI= 64 | RATING= GOOD |
| CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- | | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | 21 |
| NUMBER OF SAMPLES SURVEYED= | | 4 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | 18 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | 15.0 |

SAMPLE UNIT-14 (RANDOM) SAMPLE SIZE- 21 SLABS SAMPLE PCI- 49

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | HIGH | 21 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 7 SLABS | 33.33 | 14.2 |
| 67 LG PATCH/UTIL | MEDIUM | 1 SLABS | 4.76 | 10.6 |
| 63 LINEAR CR | LOW | 4 SLABS | 19.04 | 13.2 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 14.28 | 23.2 |
| 66 SMALL PATCH | LOW | 3 SLABS | 14.28 | 1.5 |

SAMPLE UNIT-19 (RANDOM) SAMPLE SIZE- 21 SLABS SAMPLE PCI- 80

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 21 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 11 SLABS | 52.38 | 18.4 |

| SAMPLE UNIT-3 (RANDOM) | | SAMPLE SIZE- | 18 SLABS | SAMPLE PCI- 54 |
|------------------------|----------|--------------|-------------|----------------|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 62 CORNER BR | LOW | 1 SLABS | 5.55 | 4.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 18 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 5 SLABS | 27.77 | 12.7 |
| 67 LG PATCH/UTIL | MEDIUM | 2 SLABS | 11.11 | 17.6 |
| 63 LINEAR CR | LOW | 5 SLABS | 27.77 | 16.3 |
| 66 SMALL PATCH | LOW | 7 SLABS | 38.88 | 5.5 |
| 66 SMALL PATCH | MEDIUM | 1 SLABS | 5.55 | 2.9 |

| SAMPLE UNIT-8 (RANDOM) | | SAMPLE SIZE- | 21 SLABS | SAMPLE PCI- 74 |
|------------------------|----------|--------------|-------------|----------------|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 62 CORNER BR | LOW | 1 SLABS | 4.76 | 3.7 |
| 75 CORNER SPALLING | LOW | 1 SLABS | 4.76 | 1.8 |
| 74 JOINT SPALLING | LOW | 1 SLABS | 4.76 | 2.1 |
| 65 JT SEAL DAMAGE | MEDIUM | 21 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 5 SLABS | 23.80 | 11.5 |
| 66 SMALL PATCH | LOW | 5 SLABS | 23.80 | 2.9 |
| 66 SMALL PATCH | MEDIUM | 1 SLABS | 4.76 | 2.5 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 62 CORNER BR | LOW | 11 SLABS | 2.54 | 1.9 |
| 75 CORNER SPALLING | LOW | 5 SLABS | 1.15 | 0.4 |
| 74 JOINT SPALLING | LOW | 5 SLABS | 1.15 | 0.7 |
| 65 JT SEAL DAMAGE | HIGH | 112 SLABS | 25.92 | 12.0 |
| 65 JT SEAL DAMAGE | MEDIUM | 320 SLABS | 74.07 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 149 SLABS | 34.49 | 14.6 |
| 67 LG PATCH/UTIL | MEDIUM | 16 SLABS | 3.70 | 8.8 |
| 63 LINEAR CR | LOW | 48 SLABS | 11.11 | 9.1 |
| 63 LINEAR CR | MEDIUM | 16 SLABS | 3.70 | 9.2 |
| 66 SMALL PATCH | LOW | 80 SLABS | 18.51 | 2.0 |
| 66 SMALL PATCH | MEDIUM | 11 SLABS | 2.54 | 1.3 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES - | 30.15 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES - | 28.36 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES - | 41.49 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------------|-------------------|---------|
| BRANCH NAME - | SOUTH RAMP TAXIWAY | SLAB LENGTH - | 12.5 LF |
| BRANCH NUMBER - | T22B | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 366 |

| | | | | | |
|-------------------------------------|----------|-----------|------------|----------|------|
| INSPECTION DATE - | 06/02/89 | PCI= | 58 | RATING= | GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 20 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 3 |

RECOMMENDED SAMPLES TO BE SURVEYED=

19

STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED=

18.1

SAMPLE UNIT-11 (RANDOM) SAMPLE SIZE- 18 SLABS SAMPLE PCI- 73

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.55 | 5.5 |
| 65 JT SEAL DAMAGE | HIGH | 18 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 2 SLABS | 11.11 | 9.1 |
| 73 SHRINKAGE CR | N/A | 14 SLABS | 77.77 | 11.6 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.55 | 0.7 |

SAMPLE UNIT-17 (RANDOM) SAMPLE SIZE- 18 SLABS SAMPLE PCI- 38

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | HIGH | 18 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 12 SLABS | 66.66 | 21.8 |
| 63 LINEAR CR | MEDIUM | 4 SLABS | 22.22 | 29.7 |
| 70 SCALING/CRAZING | LOW | 9 SLABS | 50.00 | 12.2 |
| 72 SHATTERED SLAB | LOW | 1 SLABS | 5.55 | 11.7 |
| 73 SHRINKAGE CR | N/A | 9 SLABS | 50.00 | 7.4 |

SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 18 SLABS SAMPLE PCI- 64

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | HIGH | 18 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 13 SLABS | 72.22 | 22.0 |
| 73 SHRINKAGE CR | N/A | 17 SLABS | 94.44 | 13.7 |
| 66 SMALL PATCH | LOW | 2 SLABS | 11.11 | 1.1 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 7 SLABS | 1.91 | 2.2 |
| 65 JT SEAL DAMAGE | HIGH | 366 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 183 SLABS | 50.00 | 20.2 |
| 63 LINEAR CR | MEDIUM | 27 SLABS | 7.37 | 15.4 |
| 70 SCALING/CRAZING | LOW | 61 SLABS | 16.66 | 6.0 |
| 72 SHATTERED SLAB | LOW | 7 SLABS | 1.91 | 4.7 |
| 73 SHRINKAGE CR | N/A | 271 SLABS | 74.04 | 11.1 |
| 66 SMALL PATCH | LOW | 20 SLABS | 5.46 | 0.7 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 55.74 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 16.60 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 27.66 PERCENT DEDUCT VALUES. |

BRANCH NAME - SOUTH OPERATIONAL APRON
BRANCH NUMBER - A01B
SECTION NUMBER - 1

SLAB LENGTH - 12.5 LF
SLAB WIDTH - 20.0 LF
NUMBER OF SLABS - 716

INSPECTION DATE - 06/02/89 PCI= 83 RATING= VERY GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 23
NUMBER OF SAMPLES SURVEYED= 3
RECOMMENDED SAMPLES TO BE SURVEYED= 15
STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 7.0

SAMPLE UNIT-12 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 82

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 67 LG PATCH/UTIL | LOW | 3 SLABS | 15.00 | 8.0 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |

SAMPLE UNIT-18 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 91

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 |

SAMPLE UNIT-6 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 77

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 63 LINEAR CR | LOW | 5 SLABS | 25.00 | 15.5 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 24 SLABS | 3.35 | 9.8 |
| 74 JOINT SPALLING | LOW | 24 SLABS | 3.35 | 1.7 |
| 67 LG PATCH/UTIL | LOW | 36 SLABS | 5.02 | 3.1 |
| 63 LINEAR CR | LOW | 72 SLABS | 10.05 | 8.5 |
| 66 SMALL PATCH | LOW | 36 SLABS | 5.02 | 0.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 35.86 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 64.14 PERCENT DEDUCT VALUES. |

BRANCH NAME - MAIN OPERATIONAL APRON SLAB LENGTH - 25.0 LF
BRANCH NUMBER - A02B SLAB WIDTH - 25.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 2592

INSPECTION DATE - 06/02/89 PCI= 45 RATING= FAIR
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 116
NUMBER OF SAMPLES SURVEYED= 12
RECOMMENDED SAMPLES TO BE SURVEYED= 42

STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED=

18.3

SAMPLE UNIT-104(RANDOM)

SAMPLE SIZE-

20 SLABS

SAMPLE PCI- 24

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | HIGH | 2 SLABS | 10.00 | 28.5 |
| 67 LG PATCH/UTIL | MEDIUM | 2 SLABS | 10.00 | 16.8 |
| 63 LINEAR CR | MEDIUM | 6 SLABS | 30.00 | 35.0 |
| 70 SCALING/CRAZING | LOW | 19 SLABS | 95.00 | 16.5 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |
| 66 SMALL PATCH | HIGH | 2 SLABS | 10.00 | 10.8 |
| 66 SMALL PATCH | MEDIUM | 4 SLABS | 20.00 | 9.5 |

SAMPLE UNIT-14 (RANDOM)

SAMPLE SIZE-

25 SLABS

SAMPLE PCI- 48

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 4.00 | 2.0 |
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | HIGH | 3 SLABS | 12.00 | 29.0 |
| 63 LINEAR CR | LOW | 4 SLABS | 16.00 | 11.8 |
| 70 SCALING/CRAZING | LOW | 25 SLABS | 100.00 | 17.0 |
| 66 SMALL PATCH | LOW | 2 SLABS | 8.00 | 1.0 |

SAMPLE UNIT-23 (RANDOM)

SAMPLE SIZE-

20 SLABS

SAMPLE PCI- 65

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 4 SLABS | 20.00 | 13.7 |
| 70 SCALING/CRAZING | LOW | 18 SLABS | 90.00 | 16.0 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 15.00 | 2.1 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |

SAMPLE UNIT-32 (RANDOM)

SAMPLE SIZE-

20 SLABS

SAMPLE PCI- 70

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 70 SCALING/CRAZING | LOW | 2 SLABS | 10.00 | 4.0 |
| 73 SHRINKAGE CR | N/A | 14 SLABS | 70.00 | 10.7 |
| 66 SMALL PATCH | LOW | 3 SLABS | 15.00 | 1.6 |

SAMPLE UNIT-41 (RANDOM)

SAMPLE SIZE-

25 SLABS

SAMPLE PCI- 21

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 4.00 | 2.0 |
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 4.00 | 2.5 |
| 63 LINEAR CR | HIGH | 5 SLABS | 20.00 | 39.4 |

| | | | | |
|--------------------|--------|----------|-------|------|
| 63 LINEAR CR | LOW | 5 SLABS | 20.00 | 13.7 |
| 63 LINEAR CR | MEDIUM | 6 SLABS | 24.00 | 31.0 |
| 70 SCALING/CRAZING | LOW | 23 SLABS | 92.00 | 16.2 |
| 73 SHRINKAGE CR | N/A | 7 SLABS | 28.00 | 3.9 |
| 66 SMALL PATCH | LOW | 6 SLABS | 24.00 | 3.0 |

| | | | | |
|------------------------|--|--------------|----------|----------------|
| SAMPLE UNIT-5 (RANDOM) | | SAMPLE SIZE- | 25 SLABS | SAMPLE PCI- 15 |
|------------------------|--|--------------|----------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 4.00 | 11.5 |
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | HIGH | 1 SLABS | 4.00 | 15.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 4.00 | 2.5 |
| 63 LINEAR CR | HIGH | 9 SLABS | 36.00 | 55.3 |
| 63 LINEAR CR | LOW | 4 SLABS | 16.00 | 11.8 |
| 63 LINEAR CR | MEDIUM | 5 SLABS | 20.00 | 28.2 |
| 70 SCALING/CRAZING | LOW | 24 SLABS | 96.00 | 16.6 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 4.00 | 1.0 |
| 66 SMALL PATCH | HIGH | 1 SLABS | 4.00 | 5.5 |

| | | | | |
|-------------------------|--|--------------|----------|----------------|
| SAMPLE UNIT-50 (RANDOM) | | SAMPLE SIZE- | 25 SLABS | SAMPLE PCI- 46 |
|-------------------------|--|--------------|----------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 4 SLABS | 16.00 | 8.5 |
| 67 LG PATCH/UTIL | MEDIUM | 1 SLABS | 4.00 | 9.5 |
| 63 LINEAR CR | HIGH | 1 SLABS | 4.00 | 14.0 |
| 63 LINEAR CR | LOW | 3 SLABS | 12.00 | 9.7 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 12.00 | 21.0 |
| 70 SCALING/CRAZING | LOW | 11 SLABS | 44.00 | 11.5 |

| | | | | |
|-------------------------|--|--------------|----------|----------------|
| SAMPLE UNIT-59 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 58 |
|-------------------------|--|--------------|----------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 3 SLABS | 15.00 | 8.0 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 |
| 70 SCALING/CRAZING | LOW | 8 SLABS | 40.00 | 11.0 |
| 73 SHRINKAGE CR | N/A | 9 SLABS | 45.00 | 6.5 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |

| | | | | |
|-------------------------|--|--------------|----------|----------------|
| SAMPLE UNIT-68 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 59 |
|-------------------------|--|--------------|----------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 2 SLABS | 10.00 | 6.0 |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 |
| 70 SCALING/CRAZING | LOW | 16 SLABS | 80.00 | 15.2 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 15.00 | 2.1 |
| 66 SMALL PATCH | LOW | 5 SLABS | 25.00 | 3.2 |

SAMPLE UNIT-77 (RANDOM)

SAMPLE SIZE-

25 SLABS

SAMPLE PCI- 37

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 4.00 | 3.0 |
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | HIGH | 2 SLABS | 8.00 | 22.3 |
| 63 LINEAR CR | LOW | 7 SLABS | 28.00 | 16.4 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 8.00 | 16.5 |
| 70 SCALING/CRAZING | LOW | 23 SLABS | 92.00 | 16.2 |
| 73 SHRINKAGE CR | N/A | 8 SLABS | 32.00 | 4.5 |
| 66 SMALL PATCH | HIGH | 1 SLABS | 4.00 | 5.5 |
| 66 SMALL PATCH | LOW | 1 SLABS | 4.00 | 0.5 |

SAMPLE UNIT-86 (RANDOM)

SAMPLE SIZE-

25 SLABS

SAMPLE PCI- 38

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 1 SLABS | 4.00 | 4.2 |
| 75 CORNER SPALLING | LOW | 1 SLABS | 4.00 | 1.5 |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 4.00 | 11.5 |
| 74 JOINT SPALLING | LOW | 1 SLABS | 4.00 | 2.0 |
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | HIGH | 2 SLABS | 8.00 | 22.3 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 8.00 | 16.5 |
| 70 SCALING/CRAZING | LOW | 24 SLABS | 96.00 | 16.6 |
| 73 SHRINKAGE CR | N/A | 8 SLABS | 32.00 | 4.5 |
| 66 SMALL PATCH | LOW | 2 SLABS | 8.00 | 1.0 |

SAMPLE UNIT-95 (RANDOM)

SAMPLE SIZE-

20 SLABS

SAMPLE PCI- 62

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |
| 70 SCALING/CRAZING | LOW | 19 SLABS | 95.00 | 16.5 |
| 66 SMALL PATCH | HIGH | 1 SLABS | 5.00 | 6.3 |
| 66 SMALL PATCH | MEDIUM | 1 SLABS | 5.00 | 2.7 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|------------|-------------|--------------|
| 62 CORNER BR | LOW | 10 SLABS | 0.38 | 0.2 |
| 75 CORNER SPALLING | HIGH | 19 SLABS | 0.73 | 0.8 |
| 75 CORNER SPALLING | LOW | 10 SLABS | 0.38 | 0.1 |
| 74 JOINT SPALLING | HIGH | 38 SLABS | 1.46 | 4.7 |
| 74 JOINT SPALLING | LOW | 58 SLABS | 2.23 | 1.3 |
| 74 JOINT SPALLING | MEDIUM | 10 SLABS | 0.38 | 0.3 |
| 65 JT SEAL DAMAGE | HIGH | 2592 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | HIGH | 29 SLABS | 1.11 | 4.4 |
| 67 LG PATCH/UTIL | LOW | 115 SLABS | 4.43 | 2.7 |
| 67 LG PATCH/UTIL | MEDIUM | 29 SLABS | 1.11 | 2.7 |
| 63 LINEAR CR | HIGH | 221 SLABS | 8.52 | 23.2 |

| | | | | |
|--------------------|--------|------------|-------|------|
| 63 LINEAR CR | LOW | 269 SLABS | 10.37 | 8.7 |
| 63 LINEAR CR | MEDIUM | 240 SLABS | 9.25 | 18.0 |
| 70 SCALING/CRAZING | LOW | 2035 SLABS | 78.51 | 15.1 |
| 73 SHRINKAGE CR | N/A | 518 SLABS | 19.98 | 2.8 |
| 66 SMALL PATCH | HIGH | 48 SLABS | 1.85 | 3.2 |
| 66 SMALL PATCH | LOW | 221 SLABS | 8.52 | 1.0 |
| 66 SMALL PATCH | MEDIUM | 48 SLABS | 1.85 | 1.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 49.02 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 11.74 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 39.24 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|---------------------------|-------------------|---------|
| BRANCH NAME - | MAIN OPER APRON-WEST EDGE | SLAB LENGTH - | 25.0 LF |
| BRANCH NUMBER - | A03B | SLAB WIDTH - | 25.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 258 |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 77 | RATING= | VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 14 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 3 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 7 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 4.0 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-12 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 79 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 8 SLABS | 40.00 | 19.0 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-3 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 72 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 5.00 | 3.3 |
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 6 SLABS | 30.00 | 13.3 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |
| 66 SMALL PATCH | MEDIUM | 1 SLABS | 5.00 | 2.7 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-7 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 79 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 2 SLABS | 10.00 | 6.0 |
| 66 SMALL PATCH | LOW | 7 SLABS | 35.00 | 5.0 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 75 CORNER SPALLING | MEDIUM | 4 SLABS | 1.55 | 1.3 |
| 74 JOINT SPALLING | LOW | 13 SLABS | 5.03 | 2.2 |
| 74 JOINT SPALLING | MEDIUM | 9 SLABS | 3.48 | 3.3 |
| 65 JT SEAL DAMAGE | MEDIUM | 258 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 34 SLABS | 13.17 | 7.2 |
| 63 LINEAR CR | LOW | 34 SLABS | 13.17 | 10.3 |
| 66 SMALL PATCH | LOW | 39 SLABS | 15.11 | 1.6 |
| 66 SMALL PATCH | MEDIUM | 4 SLABS | 1.55 | 0.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 30.56 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 20.77 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 48.66 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|-------------------------|-------------------|---------|
| BRANCH NAME - | NORTH OPERATIONAL APRON | SLAB LENGTH - | 25.0 LF |
| BRANCH NUMBER - | A04B | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 1238 |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 73 | RATING= | VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 56 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 8 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 19 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 11.0 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-19 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 50 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | MEDIUM | 2 SLABS | 10.00 | 8.0 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | HIGH | 2 SLABS | 10.00 | 26.0 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 10.00 | 19.0 |
| 70 SCALING/CRAZING | LOW | 3 SLABS | 15.00 | 5.6 |
| 73 SHRINKAGE CR | N/A | 4 SLABS | 20.00 | 2.9 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-21 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 76 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 70 SCALING/CRAZING | LOW | 16 SLABS | 80.00 | 15.2 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 15.00 | 2.1 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-30 (RANDOM) | SAMPLE SIZE- | 25 SLABS | SAMPLE PCI- | 84 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| 70 SCALING/CRAZING | LOW | 5 SLABS | 20.00 | 7.0 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 12.00 | 1.7 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-39 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 82 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---|----------|--------------|-------------|----------------|
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 70 SCALING/CRAZING | LOW | 2 SLABS | 10.00 | 4.0 |
| ----- | | | | |
| SAMPLE UNIT-44 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 79 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 5.00 | 3.3 |
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 |
| ----- | | | | |
| SAMPLE UNIT-46 (RANDOM) | | SAMPLE SIZE- | 25 SLABS | SAMPLE PCI- 74 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 4.00 | 11.5 |
| 74 JOINT SPALLING | LOW | 2 SLABS | 8.00 | 3.0 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 4.00 | 3.8 |
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 4.00 | 1.0 |
| ----- | | | | |
| SAMPLE UNIT-52 (RANDOM) | | SAMPLE SIZE- | 25 SLABS | SAMPLE PCI- 70 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | LOW | 1 SLABS | 4.00 | 1.5 |
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 4.00 | 2.2 |
| 64 DURABILITY CR | LOW | 1 SLABS | 4.00 | 1.5 |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 4.00 | 11.5 |
| 74 JOINT SPALLING | LOW | 3 SLABS | 12.00 | 4.0 |
| 74 JOINT SPALLING | MEDIUM | 2 SLABS | 8.00 | 6.8 |
| 65 JT SEAL DAMAGE | HIGH | 25 SLABS | 100.00 | 12.0 |
| ----- | | | | |
| SAMPLE UNIT-8 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 65 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 2 SLABS | 10.00 | 8.5 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 |
| 70 SCALING/CRAZING | LOW | 16 SLABS | 80.00 | 15.2 |
| 73 SHRINKAGE CR | N/A | 6 SLABS | 30.00 | 4.2 |
| ----- | | | | |
| EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION- | | | | |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 75 CORNER SPALLING | HIGH | 7 SLABS | 0.56 | 0.6 |
| 75 CORNER SPALLING | LOW | 21 SLABS | 1.69 | 0.5 |
| 75 CORNER SPALLING | MEDIUM | 14 SLABS | 1.13 | 0.9 |

| | | | | |
|--------------------|--------|------------|-------|------|
| 64 DURABILITY CR | LOW | 7 SLABS | 0.56 | 0.2 |
| 74 JOINT SPALLING | HIGH | 14 SLABS | 1.13 | 3.6 |
| 74 JOINT SPALLING | LOW | 57 SLABS | 4.60 | 2.1 |
| 74 JOINT SPALLING | MEDIUM | 35 SLABS | 2.82 | 2.7 |
| 65 JT SEAL DAMAGE | HIGH | 1096 SLABS | 88.52 | 12.0 |
| 65 JT SEAL DAMAGE | MEDIUM | 141 SLABS | 11.38 | 7.0 |
| 63 LINEAR CR | HIGH | 14 SLABS | 1.13 | 4.5 |
| 63 LINEAR CR | LOW | 14 SLABS | 1.13 | 1.1 |
| 63 LINEAR CR | MEDIUM | 21 SLABS | 1.69 | 4.2 |
| 70 SCALING/CRAZING | LOW | 297 SLABS | 23.99 | 7.9 |
| 73 SHRINKAGE CR | N/A | 127 SLABS | 10.25 | 1.5 |
| 66 SMALL PATCH | LOW | 7 SLABS | 0.56 | 0.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 20.08 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 39.34 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 40.57 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|-------------------|-------------------|---------|
| BRANCH NAME - | MAINTENANCE APRON | SLAB LENGTH - | 25.0 LF |
| BRANCH NUMBER - | A05B | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 164 |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 75 | RATING= | VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 8 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 2 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 8 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 8.4 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-3 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 69 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 64 DURABILITY CR | LOW | 1 SLABS | 5.00 | 1.8 |
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 74 JOINT SPALLING | LOW | 3 SLABS | 15.00 | 4.7 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 3 SLABS | 15.00 | 11.3 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |

| | | | | |
|------------------------|--------------|----------|-------------|--------------|
| SAMPLE UNIT-6 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 81 |
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 2 SLABS | 10.00 | 8.5 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|---------------|----------|----------|-------------|--------------|
|---------------|----------|----------|-------------|--------------|

| | | | | |
|--------------------|--------|-----------|--------|------|
| 75 CORNER SPALLING | LOW | 4 SLABS | 2.43 | 0.8 |
| 64 DURABILITY CR | LOW | 4 SLABS | 2.43 | 1.1 |
| 74 JOINT SPALLING | HIGH | 4 SLABS | 2.43 | 7.5 |
| 74 JOINT SPALLING | LOW | 12 SLABS | 7.31 | 2.8 |
| 74 JOINT SPALLING | MEDIUM | 4 SLABS | 2.43 | 2.4 |
| 65 JT SEAL DAMAGE | MEDIUM | 164 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 21 SLABS | 12.80 | 10.1 |
| 73 SHRINKAGE CR | N/A | 8 SLABS | 4.87 | 1.0 |
| 66 SMALL PATCH | LOW | 16 SLABS | 9.75 | 1.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 29.97 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 24.04 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 45.99 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------------------|-------------------|---------|
| BRANCH NAME - | SOUTH EDGE-HANGAR ACCESS | SLAB LENGTH - | 15.0 LF |
| BRANCH NUMBER - | A06B | SLAB WIDTH - | 15.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 61 |

| | | | | | |
|-------------------------------------|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 83 | RATING= | VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 3 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 1 |

RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 83 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 5.00 | 3.3 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 10.00 | 1.5 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | MEDIUM | 3 SLABS | 4.91 | 3.2 |
| 65 JT SEAL DAMAGE | MEDIUM | 61 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 3 SLABS | 4.91 | 4.8 |
| 73 SHRINKAGE CR | N/A | 6 SLABS | 9.83 | 1.4 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 29.27 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 42.68 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 28.05 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|--------------------------|-------------------|---------|
| BRANCH NAME - | HANGAR ACCESS (BLDG 119) | SLAB LENGTH - | 12.5 LF |
| BRANCH NUMBER - | A07B | SLAB WIDTH - | 15.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 206 |

 INSPECTION DATE - 06/02/89 PCI= 90 RATING= EXCELLENT
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 11
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMENDED SAMPLES TO BE SURVEYED= 10
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 2.8

SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 15 SLABS SAMPLE PCI- 88

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 6.66 | 4.9 |
| 65 JT SEAL DAMAGE | MEDIUM | 15 SLABS | 100.00 | 7.0 |

SAMPLE UNIT-9 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 92

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 75 CORNER SPALLING | MEDIUM | 6 SLABS | 2.91 | 1.9 |
| 65 JT SEAL DAMAGE | MEDIUM | 206 SLABS | 100.00 | 7.0 |
| 73 SHRINKAGE CR | N/A | 6 SLABS | 2.91 | 0.8 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 72.16 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 27.84 PERCENT DEDUCT VALUES. |

 BRANCH NAME - HANGAR ACCESS (BLDG 184) SLAB LENGTH - 20.0 LF
 BRANCH NUMBER - A09B SLAB WIDTH - 20.0 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 57

INSPECTION DATE - 06/02/89 PCI= 82 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 4
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 29 SLABS SAMPLE PCI- 85

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 2 SLABS | 6.89 | 2.6 |
| 74 JOINT SPALLING | LOW | 1 SLABS | 3.44 | 1.8 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 3.44 | 3.3 |
| 65 JT SEAL DAMAGE | MEDIUM | 29 SLABS | 100.00 | 7.0 |

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 78

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 74 JOINT SPALLING | LOW | 9 SLABS | 45.00 | 10.3 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 3 SLABS | 15.00 | 11.3 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 3 SLABS | 5.26 | 2.0 |
| 74 JOINT SPALLING | LOW | 12 SLABS | 21.05 | 6.1 |
| 65 JT SEAL DAMAGE | MEDIUM | 57 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 3 SLABS | 5.26 | 5.1 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 23.18 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 31.82 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 45.00 PERCENT DEDUCT VALUES. |

BRANCH NAME - MAINTENANCE HANGAR ACCESS SLAB LENGTH - 20.0 LF
BRANCH NUMBER - A10B SLAB WIDTH - 20.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 266

INSPECTION DATE - 06/02/89 PCI= 63 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 13
NUMBER OF SAMPLES SURVEYED= 3
RECOMMENDED SAMPLES TO BE SURVEYED= 5
STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 2.9

SAMPLE UNIT-11 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 63

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 2 SLABS | 10.00 | 9.0 |
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 5.00 | 3.3 |
| 64 DURABILITY CR | HIGH | 1 SLABS | 5.00 | 8.3 |
| 74 JOINT SPALLING | HIGH | 2 SLABS | 10.00 | 20.8 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |

SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 60

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 1 SLABS | 5.00 | 5.1 |
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 |
| 75 CORNER SPALLING | MEDIUM | 3 SLABS | 15.00 | 9.7 |
| 74 JOINT SPALLING | HIGH | 5 SLABS | 25.00 | 32.2 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |

SAMPLE UNIT-7 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 2 SLABS | 10.00 | 9.0 |
| 64 DURABILITY CR | LOW | 1 SLABS | 5.00 | 1.8 |
| 74 JOINT SPALLING | HIGH | 2 SLABS | 10.00 | 20.8 |
| 74 JOINT SPALLING | MEDIUM | 1 SLABS | 5.00 | 4.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 75 CORNER SPALLING | HIGH | 22 SLABS | 8.27 | 7.6 |
| 75 CORNER SPALLING | LOW | 4 SLABS | 1.50 | 0.5 |
| 75 CORNER SPALLING | MEDIUM | 18 SLABS | 6.76 | 5.0 |
| 64 DURABILITY CR | HIGH | 4 SLABS | 1.50 | 2.8 |
| 64 DURABILITY CR | LOW | 4 SLABS | 1.50 | 0.7 |
| 74 JOINT SPALLING | HIGH | 40 SLABS | 15.03 | 25.8 |
| 74 JOINT SPALLING | MEDIUM | 9 SLABS | 3.38 | 3.2 |
| 65 JT SEAL DAMAGE | MEDIUM | 266 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 4 SLABS | 1.50 | 1.1 |
| 73 SHRINKAGE CR | N/A | 4 SLABS | 1.50 | 0.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 19.34 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 80.66 PERCENT DEDUCT VALUES. |

 BRANCH NAME - WARM-UP PAD - TAXIWAY D SLAB LENGTH - 12.5 LF
 BRANCH NUMBER - A11B SLAB WIDTH - 20.0 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 159

INSPECTION DATE - 06/02/89 PCI= 77 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 8
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 77

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 10.00 | 1.5 |
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 8 SLABS | 5.03 | 2.2 |

| | | | | |
|-------------------|------|-----------|--------|------|
| 65 JT SEAL DAMAGE | HIGH | 159 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | LOW | 8 SLABS | 5.03 | 4.9 |
| 73 SHRINKAGE CR | N/A | 16 SLABS | 10.06 | 1.5 |
| 66 SMALL PATCH | LOW | 32 SLABS | 20.12 | 2.2 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 21.49 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 52.63 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 25.88 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|------------------------|-------------------|---------|
| BRANCH NAME - | ARM-DISARM PAD - T/W D | SLAB LENGTH - | 20.0 LF |
| BRANCH NUMBER - | A12B | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 129 |

| | | | | | |
|--|----------|-----------|------------|----------|------|
| INSPECTION DATE - | 06/02/89 | PCI= | 56 | RATING= | GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 7 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 2 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 7 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 19.0 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-3 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 69 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 2 SLABS | 10.00 | 8.5 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 15.00 | 24.0 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-5 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 42 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 5.00 | 4.0 |
| 62 CORNER BR | MEDIUM | 2 SLABS | 10.00 | 15.0 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 10.00 | 19.0 |
| 72 SHATTERED SLAB | MEDIUM | 2 SLABS | 10.00 | 27.0 |
| 73 SHRINKAGE CR | N/A | 4 SLABS | 20.00 | 2.9 |
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|-----------|-------------|--------------|
| 62 CORNER BR | LOW | 3 SLABS | 2.32 | 1.7 |
| 62 CORNER BR | MEDIUM | 6 SLABS | 4.65 | 7.6 |
| 74 JOINT SPALLING | LOW | 3 SLABS | 2.32 | 1.4 |
| 65 JT SEAL DAMAGE | MEDIUM | 129 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | LOW | 10 SLABS | 7.75 | 6.9 |
| 63 LINEAR CR | MEDIUM | 16 SLABS | 12.40 | 21.4 |
| 72 SHATTERED SLAB | MEDIUM | 6 SLABS | 4.65 | 18.4 |

| | | | | |
|-----------------|-----|----------|-------|-----|
| 73 SHRINKAGE CR | N/A | 13 SLABS | 10.07 | 1.5 |
| 66 SMALL PATCH | LOW | 13 SLABS | 10.07 | 1.1 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 83.58 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 10.45 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 5.97 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|---------------------------|-------------------|---------|
| BRANCH NAME - | ARM-DISARM ADDITION-T/W D | SLAB LENGTH - | 20.0 LF |
| BRANCH NUMBER - | A13B | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 35 |

| | | | | | |
|--|----------|-----------|------------|----------|------|
| INSPECTION DATE - | 06/02/89 | PCI= | 68 | RATING= | GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 2 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 2 |
| RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. | | | | | |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-1 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 73 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | MEDIUM | 1 SLABS | 5.00 | 8.2 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 6 SLABS | 30.00 | 13.3 |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 10.00 | 1.5 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |

| | | | | |
|------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-2 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 62 |
|------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 5.00 | 4.0 |
| 62 CORNER BR | MEDIUM | 1 SLABS | 5.00 | 8.2 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 10.00 | 19.0 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 15.00 | 2.1 |
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |
| 66 SMALL PATCH | MEDIUM | 4 SLABS | 20.00 | 9.5 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 2.85 | 2.1 |
| 62 CORNER BR | MEDIUM | 2 SLABS | 5.71 | 9.4 |
| 65 JT SEAL DAMAGE | MEDIUM | 35 SLABS | 100.00 | 7.0 |
| 67 LG PATCH/UTIL | LOW | 6 SLABS | 17.14 | 8.9 |
| 63 LINEAR CR | LOW | 1 SLABS | 2.85 | 2.8 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 5.71 | 12.7 |
| 73 SHRINKAGE CR | N/A | 4 SLABS | 11.42 | 1.6 |
| 66 SMALL PATCH | LOW | 5 SLABS | 14.28 | 1.5 |

66 SMALL PATCH MEDIUM 4 SLABS 11.42 6.1

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 51.82 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 13.44 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = 34.74 PERCENT DEDUCT VALUES.

BRANCH NAME - ARM-DISARM ADDITION-T/W A SLAB LENGTH - 20.0 LF
BRANCH NUMBER - A14B SLAB WIDTH - 20.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 39

INSPECTION DATE - 06/02/89 PCI= 69 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 2
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 15 SLABS SAMPLE PCI- 69

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 1 SLABS | 6.66 | 2.6 |
| 65 JT SEAL DAMAGE | LOW | 15 SLABS | 100.00 | 2.0 |
| 63 LINEAR CR | LOW | 1 SLABS | 6.66 | 6.2 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 6.66 | 14.2 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 6.66 | 1.1 |
| 66 SMALL PATCH | MEDIUM | 6 SLABS | 40.00 | 15.0 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 3 SLABS | 7.69 | 2.9 |
| 65 JT SEAL DAMAGE | LOW | 39 SLABS | 100.00 | 2.0 |
| 63 LINEAR CR | LOW | 3 SLABS | 7.69 | 6.8 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 7.69 | 15.9 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 7.69 | 1.2 |
| 66 SMALL PATCH | MEDIUM | 16 SLABS | 41.02 | 15.2 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 51.59 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 4.55 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = 43.86 PERCENT DEDUCT VALUES.

BRANCH NAME - ARM-DISARM PAD - T/W A SLAB LENGTH - 20.0 LF
BRANCH NUMBER - A15B SLAB WIDTH - 20.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 134

INSPECTION DATE - 06/02/89 PCI= 74 RATING= VERY GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 7
NUMBER OF SAMPLES SURVEYED= 2

RECOMMENDED SAMPLES TO BE SURVEYED= 6
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 2.1

| SAMPLE UNIT-1 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 75 |
|------------------------|----------|--------------|-------------|--------------|----|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 | |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 | |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 | |
| 72 SHATTERED SLAB | LOW | 1 SLABS | 5.00 | 10.9 | |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 | |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 | |

| SAMPLE UNIT-6 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 72 |
|------------------------|----------|--------------|-------------|--------------|----|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 75 CORNER SPALLING | LOW | 1 SLABS | 5.00 | 1.9 | |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 | |
| 63 LINEAR CR | LOW | 1 SLABS | 5.00 | 4.9 | |
| 72 SHATTERED SLAB | LOW | 1 SLABS | 5.00 | 10.9 | |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 | |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 3 SLABS | 2.23 | 0.7 |
| 65 JT SEAL DAMAGE | LOW | 67 SLABS | 50.00 | 2.0 |
| 63 LINEAR CR | HIGH | 3 SLABS | 2.23 | 8.6 |
| 63 LINEAR CR | LOW | 7 SLABS | 5.22 | 5.0 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 2.23 | 5.5 |
| 72 SHATTERED SLAB | LOW | 7 SLABS | 5.22 | 11.2 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 2.23 | 0.8 |
| 66 SMALL PATCH | LOW | 7 SLABS | 5.22 | 0.6 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 88.08 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 5.81 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 6.10 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|-------------------------|-------------------|---------|
| BRANCH NAME - | WARM-UP PAD - TAXIWAY A | SLAB LENGTH - | 12.5 LF |
| BRANCH NUMBER - | A16B | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 206 |

| | | | | | |
|--|----------|-----------|------------|----------|-----------|
| INSPECTION DATE - | 06/02/89 | PCI= | 82 | RATING= | VERY GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | 8 | | | | |
| NUMBER OF SAMPLES SURVEYED= | 2 | | | | |
| RECOMMENDED SAMPLES TO BE SURVEYED= | 8 | | | | |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | 6.3 | | | | |

| SAMPLE UNIT-2 (RANDOM) | | SAMPLE SIZE- | 15 SLABS | SAMPLE PCI- | 86 |
|------------------------|--|--------------|----------|-------------|----|
|------------------------|--|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 6.66 | 5.4 |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 6.66 | 4.1 |
| 63 LINEAR CR | LOW | 1 SLABS | 6.66 | 6.2 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 6.66 | 1.1 |
| 66 SMALL PATCH | LOW | 1 SLABS | 6.66 | 0.8 |

SAMPLE UNIT-7 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 77

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|----------------|----------|----------|-------------|--------------|
| 63 LINEAR CR | LOW | 2 SLABS | 10.00 | 8.5 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 10.00 | 19.0 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 6 SLABS | 2.91 | 2.1 |
| 67 LG PATCH/UTIL | LOW | 6 SLABS | 2.91 | 1.9 |
| 63 LINEAR CR | LOW | 18 SLABS | 8.73 | 7.6 |
| 63 LINEAR CR | MEDIUM | 12 SLABS | 5.82 | 12.9 |
| 73 SHRINKAGE CR | N/A | 6 SLABS | 2.91 | 0.8 |
| 66 SMALL PATCH | LOW | 18 SLABS | 8.73 | 1.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 85.93 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | .00 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 14.07 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|-------------------------|-------------------|---------|
| BRANCH NAME - | WARM-UP PAD - TAXIWAY C | SLAB LENGTH - | 12.5 LF |
| BRANCH NUMBER - | A18B | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 159 |

| | | | | | |
|--|----------|-----------|------------|----------|------|
| INSPECTION DATE - | 06/02/89 | PCI= | 69 | RATING= | GOOD |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 8 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 2 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 8 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 3.5 |

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 5.00 | 4.0 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 2 SLABS | 10.00 | 6.0 |
| 63 LINEAR CR | LOW | 5 SLABS | 25.00 | 15.5 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |
| 66 SMALL PATCH | LOW | 8 SLABS | 40.00 | 5.7 |
| 66 SMALL PATCH | MEDIUM | 2 SLABS | 10.00 | 5.5 |

| SAMPLE UNIT-6 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 71 | |
|------------------------|----------|--------------|-------------|----------------|--|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 62 CORNER BR | LOW | 1 SLABS | 5.00 | 4.0 | |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 | |
| 67 LG PATCH/UTIL | LOW | 1 SLABS | 5.00 | 3.1 | |
| 63 LINEAR CR | LOW | 4 SLABS | 20.00 | 13.7 | |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 | |
| 66 SMALL PATCH | LOW | 3 SLABS | 15.00 | 1.6 | |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
|-------------------|----------|-----------|-------------|--------------|--|
| 62 CORNER BR | LOW | 8 SLABS | 5.03 | 4.0 | |
| 65 JT SEAL DAMAGE | HIGH | 159 SLABS | 100.00 | 12.0 | |
| 67 LG PATCH/UTIL | LOW | 12 SLABS | 7.54 | 4.7 | |
| 63 LINEAR CR | LOW | 36 SLABS | 22.64 | 14.6 | |
| 73 SHRINKAGE CR | N/A | 8 SLABS | 5.03 | 1.0 | |
| 66 SMALL PATCH | LOW | 44 SLABS | 27.67 | 3.6 | |
| 66 SMALL PATCH | MEDIUM | 8 SLABS | 5.03 | 2.7 | |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 43.66 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 28.17 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = 28.17 PERCENT DEDUCT VALUES.

 BRANCH NAME - NORTH CALIBRATION PAD SLAB LENGTH - 25.0 LF
 BRANCH NUMBER - A26B SLAB WIDTH - 25.0 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 97

INSPECTION DATE - 06/02/89 PCI= 42 RATING= FAIR
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 5
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

| SAMPLE UNIT-2 (RANDOM) | | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 44 | |
|------------------------|----------|--------------|-------------|----------------|--|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 63 LINEAR CR | LOW | 2 SLABS | 10.00 | 8.5 | |
| 63 LINEAR CR | MEDIUM | 5 SLABS | 25.00 | 31.8 | |
| 72 SHATTERED SLAB | LOW | 4 SLABS | 20.00 | 26.5 | |
| 73 SHRINKAGE CR | N/A | 14 SLABS | 70.00 | 10.7 | |
| 66 SMALL PATCH | LOW | 3 SLABS | 15.00 | 1.6 | |

| SAMPLE UNIT-5 (RANDOM) | | SAMPLE SIZE- | 19 SLABS | SAMPLE PCI- 40 | |
|------------------------|----------|--------------|-------------|----------------|--|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE | |
| 65 JT SEAL DAMAGE | LOW | 19 SLABS | 100.00 | 2.0 | |
| 67 LG PATCH/UTIL | LOW | 4 SLABS | 21.05 | 10.4 | |

| | | | | |
|-------------------|--------|----------|-------|------|
| 63 LINEAR CR | HIGH | 1 SLABS | 5.26 | 16.8 |
| 63 LINEAR CR | LOW | 9 SLABS | 47.36 | 19.9 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.26 | 12.0 |
| 72 SHATTERED SLAB | LOW | 2 SLABS | 10.52 | 18.2 |
| 73 SHRINKAGE CR | N/A | 15 SLABS | 78.94 | 11.8 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | LOW | 47 SLABS | 48.45 | 2.0 |
| 67 LG PATCH/UTIL | LOW | 10 SLABS | 10.30 | 6.1 |
| 63 LINEAR CR | HIGH | 2 SLABS | 2.06 | 8.1 |
| 63 LINEAR CR | LOW | 27 SLABS | 27.83 | 16.3 |
| 63 LINEAR CR | MEDIUM | 15 SLABS | 15.46 | 24.4 |
| 72 SHATTERED SLAB | LOW | 15 SLABS | 15.46 | 22.7 |
| 73 SHRINKAGE CR | N/A | 72 SLABS | 74.22 | 11.1 |
| 66 SMALL PATCH | LOW | 7 SLABS | 7.21 | 0.9 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 78.06 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 2.18 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 19.76 PERCENT DEDUCT VALUES. |

 BRANCH NAME - WASHRACK AND ACCESS SLAB LENGTH - 12.5 LF
 BRANCH NUMBER - A27B SLAB WIDTH - 15.0 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 134

INSPECTION DATE - 06/02/89 PCI= 92 RATING= EXCELLENT
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 6
 NUMBER OF SAMPLES SURVEYED= 2
 RECOMMENDED SAMPLES TO BE SURVEYED= 6
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 2.1

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 90

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 3 SLABS | 15.00 | 5.5 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 73 SHRINKAGE CR | N/A | 1 SLABS | 5.00 | 1.0 |

 SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 16 SLABS SAMPLE PCI- 93

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | MEDIUM | 16 SLABS | 100.00 | 7.0 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 11 SLABS | 8.20 | 3.0 |
| 65 JT SEAL DAMAGE | MEDIUM | 134 SLABS | 100.00 | 7.0 |

73 SHRINKAGE CR N/A 4 SLABS 2.98 0.8

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 64.81 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = 35.19 PERCENT DEDUCT VALUES.

BRANCH NAME - POWER CHECK PAD SLAB LENGTH - 25.0 LF
BRANCH NUMBER - A28B SLAB WIDTH - 20.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 34

INSPECTION DATE - 06/02/89 PCI= 56 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
TOTAL NUMBER OF SAMPLES IN SECTION= 2
NUMBER OF SAMPLES SURVEYED= 1
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 56

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 5.00 | 4.0 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 4 SLABS | 20.00 | 10.0 |
| 63 LINEAR CR | LOW | 17 SLABS | 85.00 | 22.0 |
| 63 LINEAR CR | MEDIUM | 1 SLABS | 5.00 | 11.6 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 15.00 | 2.1 |
| 66 SMALL PATCH | LOW | 2 SLABS | 10.00 | 1.1 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 2 SLABS | 5.88 | 4.8 |
| 65 JT SEAL DAMAGE | HIGH | 34 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 7 SLABS | 20.58 | 10.2 |
| 63 LINEAR CR | LOW | 29 SLABS | 85.29 | 22.0 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 5.88 | 13.0 |
| 73 SHRINKAGE CR | N/A | 5 SLABS | 14.70 | 2.0 |
| 66 SMALL PATCH | LOW | 3 SLABS | 8.82 | 1.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 61.23 PERCENT DEDUCT VALUES.
CLIMATE/DURABILITY RELATED DISTRESSES = 18.46 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = 20.31 PERCENT DEDUCT VALUES.

BRANCH NAME - POWER CHECK PAD SLAB LENGTH - 25.0 LF
BRANCH NUMBER - A29B SLAB WIDTH - 25.0 LF
SECTION NUMBER - 1 NUMBER OF SLABS - 22

INSPECTION DATE - 06/02/89 PCI= 68 RATING= GOOD
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-

TOTAL NUMBER OF SAMPLES IN SECTION= 1
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

| SAMPLE UNIT-1 (RANDOM) | SAMPLE SIZE- | 16 SLABS | SAMPLE PCI- | 68 |
|------------------------|--------------|----------|-------------|--------------|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 62 CORNER BR | LOW | 1 SLABS | 6.25 | 5.1 |
| 65 JT SEAL DAMAGE | HIGH | 16 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 3 SLABS | 18.75 | 9.5 |
| 63 LINEAR CR | LOW | 1 SLABS | 6.25 | 5.9 |
| 72 SHATTERED SLAB | LOW | 1 SLABS | 6.25 | 12.8 |
| 73 SHRINKAGE CR | N/A | 2 SLABS | 12.50 | 1.8 |
| 66 SMALL PATCH | LOW | 1 SLABS | 6.25 | 0.8 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 4.54 | 3.5 |
| 65 JT SEAL DAMAGE | HIGH | 22 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 4 SLABS | 18.18 | 9.3 |
| 63 LINEAR CR | LOW | 1 SLABS | 4.54 | 4.4 |
| 72 SHATTERED SLAB | LOW | 1 SLABS | 4.54 | 10.1 |
| 73 SHRINKAGE CR | N/A | 3 SLABS | 13.63 | 1.9 |
| 66 SMALL PATCH | LOW | 1 SLABS | 4.54 | 0.5 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***
 LOAD RELATED DISTRESSES = 43.17 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 28.78 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = 28.06 PERCENT DEDUCT VALUES.

| | |
|-------------------------------|-----------------------|
| BRANCH NAME - POWER CHECK PAD | SLAB LENGTH - 25.0 LF |
| BRANCH NUMBER - A30B | SLAB WIDTH - 25.0 LF |
| SECTION NUMBER - 1 | NUMBER OF SLABS - 30 |

INSPECTION DATE - 06/02/89 PCI= 61 RATING= GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 2
 NUMBER OF SAMPLES SURVEYED= 1
 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.

| SAMPLE UNIT-1 (RANDOM) | SAMPLE SIZE- | 14 SLABS | SAMPLE PCI- | 61 |
|------------------------|--------------|----------|-------------|--------------|
| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
| 74 JOINT SPALLING | LOW | 1 SLABS | 7.14 | 2.7 |
| 65 JT SEAL DAMAGE | HIGH | 14 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 7 SLABS | 50.00 | 18.0 |
| 67 LG PATCH/UTIL | MEDIUM | 1 SLABS | 7.14 | 13.9 |
| 63 LINEAR CR | LOW | 1 SLABS | 7.14 | 6.5 |
| 66 SMALL PATCH | LOW | 3 SLABS | 21.42 | 2.4 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | LOW | 2 SLABS | 6.66 | 2.6 |
| 65 JT SEAL DAMAGE | HIGH | 30 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | LOW | 15 SLABS | 50.00 | 18.0 |
| 67 LG PATCH/UTIL | MEDIUM | 2 SLABS | 6.66 | 13.3 |
| 63 LINEAR CR | LOW | 2 SLABS | 6.66 | 6.2 |
| 66 SMALL PATCH | LOW | 6 SLABS | 20.00 | 2.2 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 11.42 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 22.10 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 66.48 PERCENT DEDUCT VALUES. |

| | | | |
|------------------|------------|-------------------|---------|
| BRANCH NAME - | SOUTH RAMP | SLAB LENGTH - | 12.5 LF |
| BRANCH NUMBER - | A31B | SLAB WIDTH - | 20.0 LF |
| SECTION NUMBER - | 1 | NUMBER OF SLABS - | 854 |

| | | | | | |
|--|----------|-----------|------------|----------|------|
| INSPECTION DATE - | 06/02/89 | PCI= | 31 | RATING= | POOR |
| CONDITION- RIDING- | SAFETY- | DRAINAGE- | SHOULDERS- | OVERALL- | |
| TOTAL NUMBER OF SAMPLES IN SECTION= | | | | | 45 |
| NUMBER OF SAMPLES SURVEYED= | | | | | 5 |
| RECOMMENDED SAMPLES TO BE SURVEYED= | | | | | 18 |
| STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= | | | | | 9.5 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-14 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- | 20 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 |
| 63 LINEAR CR | LOW | 12 SLABS | 60.00 | 21.2 |
| 63 LINEAR CR | MEDIUM | 5 SLABS | 25.00 | 31.8 |
| 70 SCALING/CRAZING | HIGH | 1 SLABS | 5.00 | 14.5 |
| 70 SCALING/CRAZING | LOW | 3 SLABS | 15.00 | 5.6 |
| 72 SHATTERED SLAB | LOW | 1 SLABS | 5.00 | 10.9 |
| 73 SHRINKAGE CR | N/A | 15 SLABS | 75.00 | 11.2 |
| 66 SMALL PATCH | HIGH | 1 SLABS | 5.00 | 6.3 |
| 66 SMALL PATCH | LOW | 4 SLABS | 20.00 | 2.2 |

| | | | | |
|-------------------------|--------------|----------|-------------|----|
| SAMPLE UNIT-30 (RANDOM) | SAMPLE SIZE- | 16 SLABS | SAMPLE PCI- | 41 |
|-------------------------|--------------|----------|-------------|----|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 65 JT SEAL DAMAGE | HIGH | 16 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | HIGH | 1 SLABS | 6.25 | 18.9 |
| 63 LINEAR CR | LOW | 8 SLABS | 50.00 | 20.2 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 18.75 | 27.2 |
| 73 SHRINKAGE CR | N/A | 10 SLABS | 62.50 | 9.5 |

| | | | |
|-------------------------|--------------|----------|----------------|
| SAMPLE UNIT-35 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 36 |
|-------------------------|--------------|----------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 65 JT SEAL DAMAGE | MEDIUM | 20 SLABS | 100.00 | 7.0 |
| 63 LINEAR CR | HIGH | 2 SLABS | 10.00 | 26.0 |
| 63 LINEAR CR | LOW | 11 SLABS | 55.00 | 21.0 |
| 63 LINEAR CR | MEDIUM | 3 SLABS | 15.00 | 24.0 |
| 73 SHRINKAGE CR | N/A | 11 SLABS | 55.00 | 8.0 |

| | | | |
|------------------------|--------------|----------|----------------|
| SAMPLE UNIT-4 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 21 |
|------------------------|--------------|----------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 1 SLABS | 5.00 | 13.2 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 |
| 63 LINEAR CR | LOW | 14 SLABS | 70.00 | 22.0 |
| 63 LINEAR CR | MEDIUM | 2 SLABS | 10.00 | 19.0 |
| 70 SCALING/CRAZING | HIGH | 3 SLABS | 15.00 | 34.0 |
| 72 SHATTERED SLAB | LOW | 1 SLABS | 5.00 | 10.9 |
| 73 SHRINKAGE CR | N/A | 17 SLABS | 85.00 | 12.7 |
| 66 SMALL PATCH | LOW | 5 SLABS | 25.00 | 3.2 |

| | | | |
|-------------------------|--------------|----------|----------------|
| SAMPLE UNIT-40 (RANDOM) | SAMPLE SIZE- | 20 SLABS | SAMPLE PCI- 35 |
|-------------------------|--------------|----------|----------------|

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|-------------------|----------|----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 2 SLABS | 10.00 | 20.8 |
| 74 JOINT SPALLING | LOW | 1 SLABS | 5.00 | 2.2 |
| 65 JT SEAL DAMAGE | LOW | 20 SLABS | 100.00 | 2.0 |
| 63 LINEAR CR | HIGH | 1 SLABS | 5.00 | 16.2 |
| 63 LINEAR CR | LOW | 14 SLABS | 70.00 | 22.0 |
| 63 LINEAR CR | MEDIUM | 4 SLABS | 20.00 | 28.2 |
| 73 SHRINKAGE CR | N/A | 10 SLABS | 50.00 | 7.4 |

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 74 JOINT SPALLING | HIGH | 45 SLABS | 5.26 | 13.7 |
| 74 JOINT SPALLING | LOW | 9 SLABS | 1.05 | 0.6 |
| 65 JT SEAL DAMAGE | HIGH | 498 SLABS | 58.31 | 12.0 |
| 65 JT SEAL DAMAGE | LOW | 178 SLABS | 20.84 | 2.0 |
| 65 JT SEAL DAMAGE | MEDIUM | 178 SLABS | 20.84 | 7.0 |
| 63 LINEAR CR | HIGH | 53 SLABS | 6.20 | 18.8 |
| 63 LINEAR CR | LOW | 525 SLABS | 61.47 | 21.3 |
| 63 LINEAR CR | MEDIUM | 151 SLABS | 17.68 | 26.3 |
| 70 SCALING/CRAZING | HIGH | 36 SLABS | 4.21 | 12.5 |
| 70 SCALING/CRAZING | LOW | 27 SLABS | 3.16 | 1.4 |
| 72 SHATTERED SLAB | LOW | 18 SLABS | 2.10 | 5.2 |
| 73 SHRINKAGE CR | N/A | 561 SLABS | 65.69 | 10.0 |
| 66 SMALL PATCH | HIGH | 9 SLABS | 1.05 | 1.8 |
| 66 SMALL PATCH | LOW | 80 SLABS | 9.36 | 1.0 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 53.59 PERCENT DEDUCT VALUES.
 CLIMATE/DURABILITY RELATED DISTRESSES = 15.72 PERCENT DEDUCT VALUES.
 OTHER RELATED DISTRESSES = 30.69 PERCENT DEDUCT VALUES.

 BRANCH NAME - MAIN OPER APRON ROADWAY SLAB LENGTH - 15.0 LF
 BRANCH NUMBER - ROAD SLAB WIDTH - 20.0 LF
 SECTION NUMBER - 1 NUMBER OF SLABS - 354

INSPECTION DATE - 06/02/89 PCI= 74 RATING= VERY GOOD
 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-
 TOTAL NUMBER OF SAMPLES IN SECTION= 18
 NUMBER OF SAMPLES SURVEYED= 3
 RECOMMENDED SAMPLES TO BE SURVEYED= 13
 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 7.5

SAMPLE UNIT-15 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 2 SLABS | 10.00 | 3.7 |
| 75 CORNER SPALLING | MEDIUM | 2 SLABS | 10.00 | 7.1 |
| 74 JOINT SPALLING | LOW | 7 SLABS | 35.00 | 8.8 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |
| 67 LG PATCH/UTIL | MEDIUM | 1 SLABS | 5.00 | 11.0 |
| 66 SMALL PATCH | LOW | 1 SLABS | 5.00 | 0.6 |
| 66 SMALL PATCH | MEDIUM | 3 SLABS | 15.00 | 7.6 |

 SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 74

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 62 CORNER BR | LOW | 1 SLABS | 5.00 | 4.0 |
| 75 CORNER SPALLING | LOW | 2 SLABS | 10.00 | 3.7 |
| 75 CORNER SPALLING | MEDIUM | 1 SLABS | 5.00 | 3.3 |
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |

 SAMPLE UNIT-9 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 81

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|----------|-------------|--------------|
| 75 CORNER SPALLING | LOW | 2 SLABS | 10.00 | 3.7 |
| 74 JOINT SPALLING | LOW | 2 SLABS | 10.00 | 3.5 |
| 65 JT SEAL DAMAGE | HIGH | 20 SLABS | 100.00 | 12.0 |

 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

| DISTRESS TYPE | SEVERITY | QUANTITY | DENSITY-PCT | DEDUCT-VALUE |
|--------------------|----------|-----------|-------------|--------------|
| 62 CORNER BR | LOW | 6 SLABS | 1.69 | 1.2 |
| 75 CORNER SPALLING | LOW | 35 SLABS | 9.88 | 3.6 |
| 75 CORNER SPALLING | MEDIUM | 18 SLABS | 5.08 | 3.4 |
| 74 JOINT SPALLING | LOW | 65 SLABS | 18.36 | 5.5 |
| 65 JT SEAL DAMAGE | HIGH | 354 SLABS | 100.00 | 12.0 |

| | | | | |
|------------------|--------|----------|------|-----|
| 67 LG PATCH/UTIL | MEDIUM | 6 SLABS | 1.69 | 4.2 |
| 66 SMALL PATCH | LOW | 6 SLABS | 1.69 | 0.2 |
| 66 SMALL PATCH | MEDIUM | 18 SLABS | 5.08 | 2.7 |

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

| | | |
|--------------------|----------------------|------------------------------|
| LOAD | RELATED DISTRESSES = | 3.66 PERCENT DEDUCT VALUES. |
| CLIMATE/DURABILITY | RELATED DISTRESSES = | 36.59 PERCENT DEDUCT VALUES. |
| OTHER | RELATED DISTRESSES = | 59.76 PERCENT DEDUCT VALUES. |